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**Logan Colbert**  
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PALMER ENGINEERING COMPANY  
2817 ERICA PLACE  
NASHVILLE, TN 37204  
LOGAN COLBERT, P.E. NO. 117913

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET .....	STRUCTURE-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX AND STANDARD DRAWINGS.....	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES.....	2
GENERAL AND SPECIAL NOTES .....	2A
ENVIRONMENTAL NOTES.....	2E
EPSC PLAN AND NOTES.....	2E1-2E3
UTILITY NOTES AND UTILITY OWNERS.....	3
TRAFFIC CONTROL PLANS AND NOTES .....	T1-T2
BRIDGE INDEX & LAYOUT OF BRIDGES TO BE REPAIRED .....	B1 (BR-133-330)
ESTIMATED BRIDGE QUANTITIES AND BRIDGE NOTES.....	B2 (BR-133-331)
BEAM REPAIRS .....	B3 (BR-133-332)
CONCRETE REPAIRS (WBL).....	B4 (BR-133-333)
CONCRETE REPAIRS (EBL).....	B5 (BR-133-334)
FRP REPAIR DETAILS .....	B6 (BR-133-335)
CONCRETE REPAIR DETAILS .....	B7 (BR-133-336)

YEAR	PROJECT NO.	SHEET NO.
2026	181040-M3-010	STRUCTURE-SIGN1

**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

**SIGNATURE  
SHEET**

Index Of Sheets  
SEE SHEET 1A

# STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF ENGINEERING

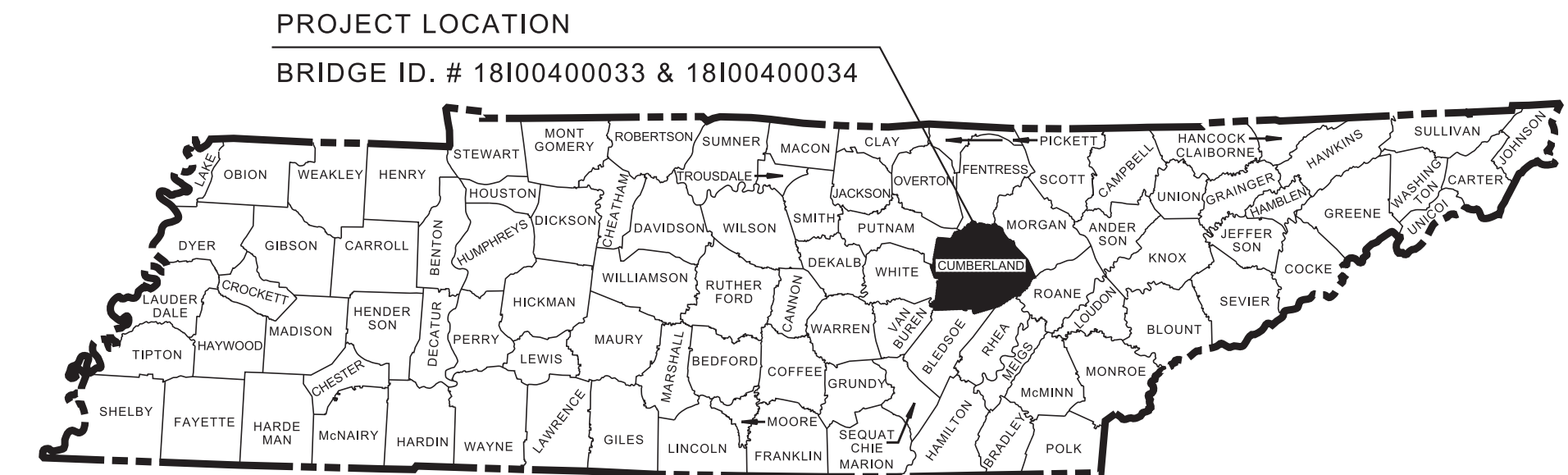
DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO
RAILROAD INVOLVEMENT	YES	NO X

TENN.	YEAR	SHEET NO.
	2026	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	181040-M3-010	

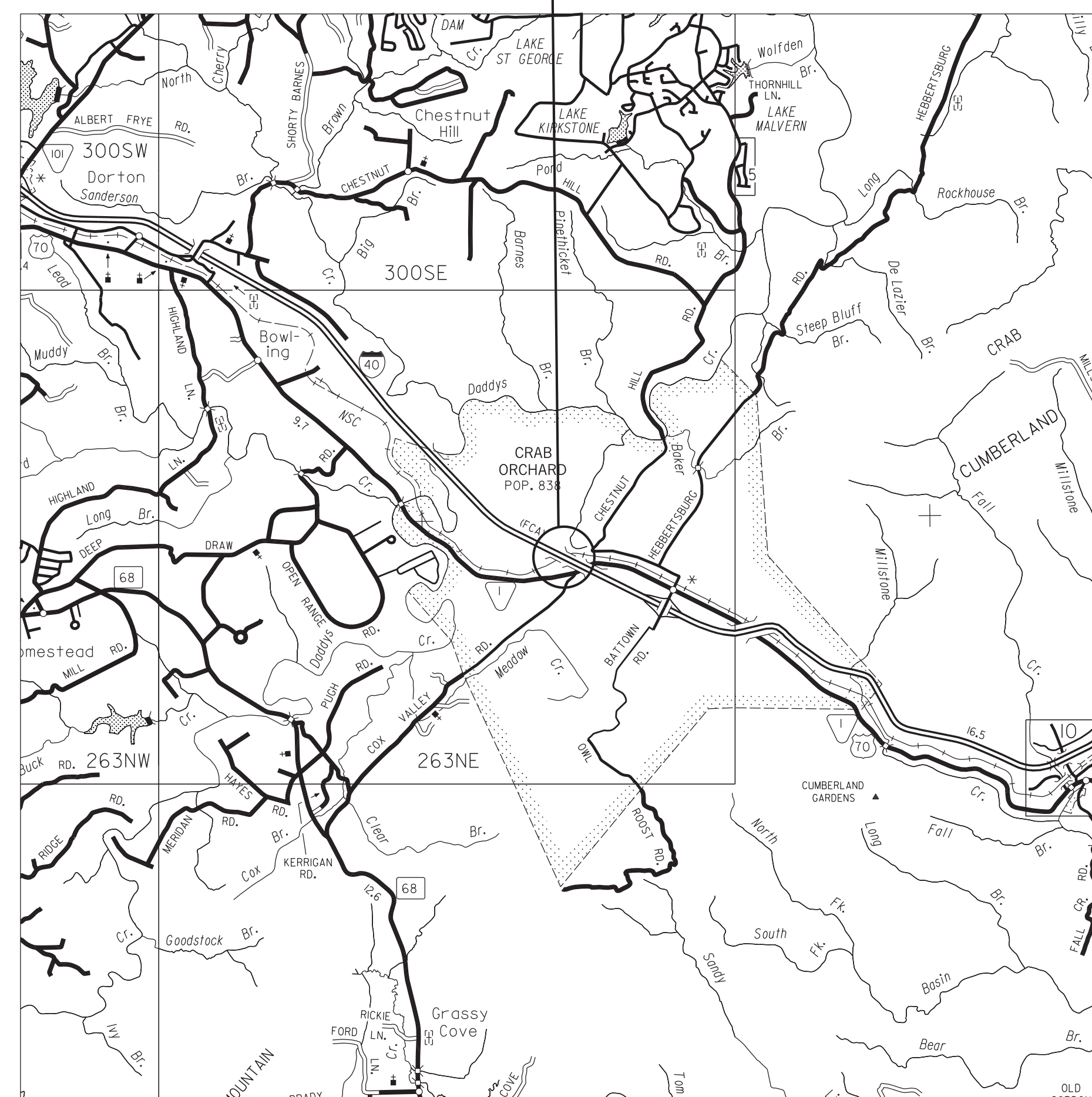
## CUMBERLAND COUNTY

BRIDGES OVER ABANDONED SOUTHERN RAILWAY AND MAIN STREET, LM 23.92

### PS&E BRIDGE REPAIR INTERSTATE 40



BRIDGE NO: 18100400033 & 18100400034



SCALE: 1"= 1 MILE

#### SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT REPAIR OFFICE PROJECT MANAGER : SEAN MONTGOMERY

DESIGNED BY : PALMER ENGINEERING COMPANY

DESIGNER : LOGAN COLBERT, P.E. CHECKED BY : SCOTT WILSON, P.E.

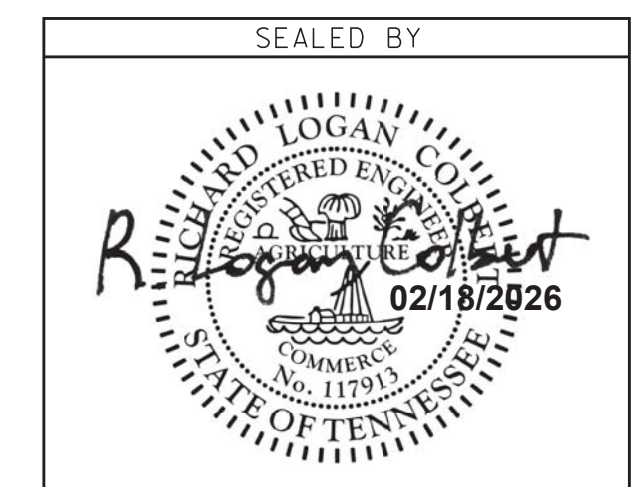
P.E. NO. 181040-M3-010

PIN NO. 136145.00

ROADWAY LENGTH	0.038 MILES
BRIDGE LENGTH	0.038 MILES
PROJECT LENGTH	0.038 MILES

I-40	
AADT (2023)	34,465
POSTED SPEED	70 MPH

TOTAL DISTURBED AREA < 0.95 ACRE



APPROVED:   
SHANE HESTER, CHIEF ENGINEER

DATE: \_\_\_\_\_

APPROVED:   
WILL REID, COMMISSIONER

# ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEET .....	STRUCTURE-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX AND STANDARD DRAWINGS .....	1A
PROJECT COMMITMENTS .....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
GENERAL AND SPECIAL NOTES .....	2A
ENVIRONMENTAL NOTES .....	2E
EPSC PLAN AND NOTES .....	2E1-2E3
UTILITY NOTES AND UTILITY OWNERS .....	3
TRAFFIC CONTROL PLANS AND NOTES .....	T1-T2
BRIDGE PLANS .....	B1-B7

NO UTILITY SHEETS ARE INCLUDED IN THIS PLAN SET.

# STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
<b>STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS</b>		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	01-30-26	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
RD-L-5	07-30-24	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL

## ROADWAY, PAVEMENT APPURTENANCES, AND FENCES

S-F-1	03-01-23	HIGH VISIBILITY FENCE
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## EROSION PREVENTION AND SEDIMENT CONTROL

EC-STR-8	06-10-14	FILTER SOCK
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# STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
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## SIGNS

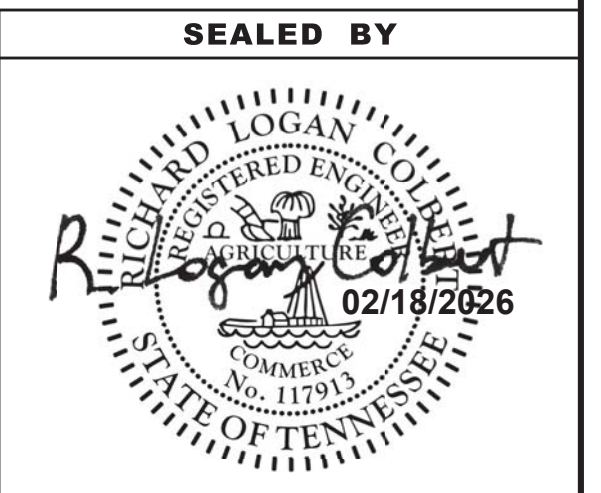
T-S-9	07-30-25	STANDARD LAYOUT GROUND MOUNTED SIGNS
T-S-10	07-30-25	STANDARD MOUNTING DETAILS FLAT SHEET SIGNS ALUMINUM-STEEL DESIGN
T-S-20	07-30-25	SIGN DETAILS

## DESIGN – TRAFFIC CONTROL

T-WZ-12	03-26-25	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-32	03-26-25	TRAFFIC CONTROL PLAN SIGNAL LAYOUT FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-33	03-26-25	TRAFFIC CONTROL PLAN FOR CLOSE INTERSECTION CONDITIONS USING TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-34	03-26-25	TRAFFIC CONTROL PLAN GENERAL NOTES FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-35	03-26-25	TRAFFIC CONTROL PLAN PAY ITEM AND SIGN DETAILS FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	1A
PS&E	2026	181040-M3-010	1A

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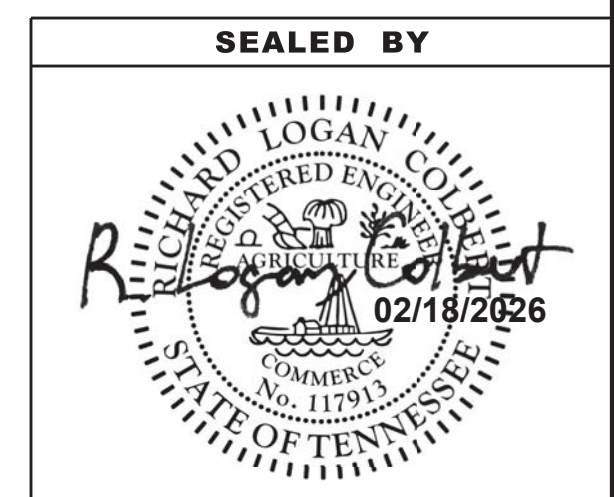
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX AND  
STANDARD DRAWINGS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	1B
PS&E	2026	181040-M3-010	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISION	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON THE FOLLOWING BRIDGES AND NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03). • BRIDGE NO. 18100400033 I-40 EB OVER NFA A244 LM 23.92 (18-10040-23.92R) • BRIDGE NO. 18100400034 I-40 WB OVER NFA A244 LM 23.92 (18-10040-23.92L)	ENTIRE BRIDGES



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PROJECT  
COMMITMENTS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	2A
PS&E	2026	181040-M3-010	2

### ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
209-03.20	FILTER SOCK (8 INCH)	L.F.	800
707-08.11	HIGH-VISIBILITY CONSTRUCTION FENCE	L.F.	400
(1) 712-01	TRAFFIC CONTROL	LS	1
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	82
(2) 712-06	SIGNS (CONSTRUCTION)	S.F.	468
712-08.10	MOBILE MESSAGE SIGN UNIT W/ ATTENUATOR	HOURL	110
(3) 712-08.11	QUEUE PROTECTION TRUCK	HOURL	110
712-09.04	REMOVABLE PAVEMENT MARKING (STOP LINE)	L.F.	48
712-09.09	REMOVABLE PAVEMENT MARKING (4" LINE)	L.F.	5000
(4) 713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	1
717-01	MOBILIZATION	LS	1
730-40.02	TEMPORARY TRAFFIC SIGNAL SYSTEM	LS	1

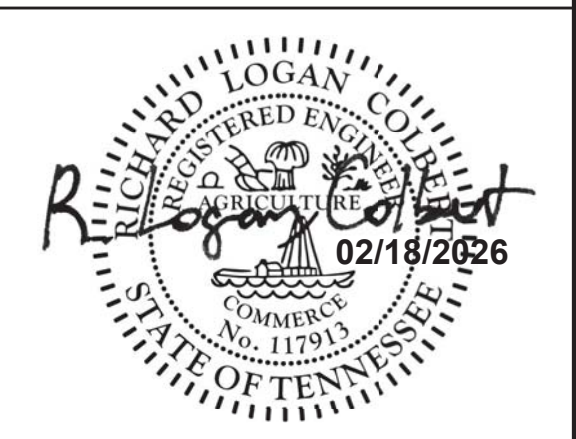
### FOOTNOTES

(1)	ALL COSTS ASSOCIATED WITH INSTALLING, STORING, AND RE-INSTALLING ALL TRAFFIC CONTROL DEVICES DURING AND BETWEEN THE DIFFERENT TRAFFIC CONTROL PHASES SHALL BE INCLUDED IN THE PRICE BID FOR EACH ITEM. DURING THE TIME BETWEEN DIFFERENT TRAFFIC CONTROL PHASES, THE CONTRACTOR SHALL STORE ALL TRAFFIC CONTROL DEVICES IN A PROPER LOCATION THAT WILL NOT INTERFERE WITH THE TRAFFIC FLOW. ALL WORK MUST MEET THE FULL APPROVAL OF THE TDOT ENGINEER.
(2)	ANY LOSS OR DAMAGE TO THE SIGNS SHALL BE PAID FOR BY THE CONTRACTOR.
(3)	SEE SPECIAL PROVISION SP712PTQ. QUANTITY ASSUMES 2 TRUCKS IN WESTBOUND DIRECTION OF I-40. ALSO SEE SPECIAL PROVISION SP108B FOR WORK HOUR RESTRICTIONS.
(4)	CHANGEABLE MESSAGE SIGN UNIT TO BE USED AT ENGINEER'S DISCRETION. LOCATION AND MESSAGE TO BE DETERMINED BY THE ENGINEER.

### TRAFFIC CONTROL SIGN QUANTITIES

TYPE	DESCRIPTION	SIZE (INCHES)		S.F. (EACH)	Main St.	I-40	SIGNS (CONST.) 712-06, S.F.
		L	x W				
G20-2	END ROAD WORK	36	x 18	4.5	4		18
G20-2	END ROAD WORK	48	x 24	8		2	16
R10-6	STOP HERE ON RED	24	x 36	6	4		24
R10-6(MOD)	STAY IN LANE TO EXTEND GREEN	30	x 42	8.75	4		35
SPECIAL	MAXIMUM XX MINUTE RED	30	x 30	6.25	4		25
SPECIAL	MAINTAIN XX MPH SPEED	42	x 36	10.5	4		42
W1-4R	REVERSE CURVE (RIGHT)	36	x 36	9	1		9
W20-1	ROAD WORK AHEAD	36	x 36	9	4		36
W20-1	ROAD WORK 1000 FT	36	x 36	9	3		27
W20-4	ONE LANE ROAD AHEAD	36	x 36	9	4		36
W20-5L	LEFT LANE CLOSED (1 MILE)	48	x 48	16		2	32
W20-5L	LEFT LANE CLOSED (1/2 MILE)	48	x 48	16		2	32
W20-5L	LEFT LANE CLOSED (1500 FT)	48	x 48	16		2	32
W4-2L	LEFT LANE ENDS	48	x 48	16		2	32
W3-3	SIGNAL AHEAD (STOP)	36	x 36	9	4		36
W3-4	BE PREPARED TO STOP	36	x 36	9	4		36
<b>TOTAL</b>							<b>468</b>

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ESTIMATED  
ROADWAY  
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	2B
PS&E	2026	181040-M3-010	2A

# GENERAL NOTES

## GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

## MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

## CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (4) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (5) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (6) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (7) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (8) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (9) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (10) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (11) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

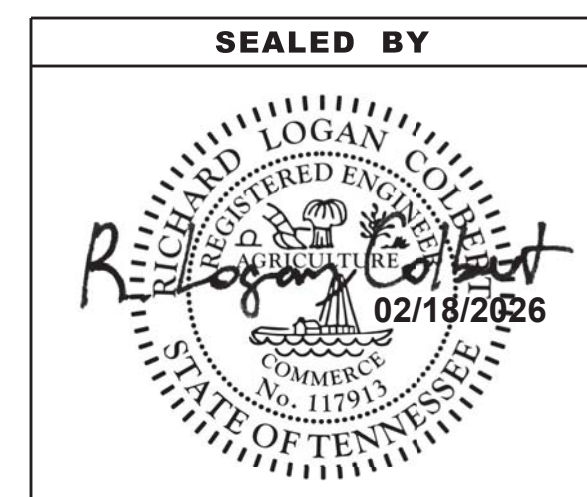
# SPECIAL NOTES

## DEMOLITION

### DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (1) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (2) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

GENERAL AND  
SPECIAL NOTES

# ENVIRONMENTAL NOTES

## ENVIRONMENTAL GENERAL NOTES

### NATURAL RESOURCES

- (1) SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT NATURAL RESOURCES AND WATER QUALITY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG NATURAL RESOURCES IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS, WETLANDS OR OTHER NATURAL FEATURES IN ACCORDANCE WITH TDOT STANDARDS. EPSC MEASURES SHALL BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (2) NEW CHANNEL CONSTRUCTION SHALL BE COMPLETED IN THE DRY AND STABILIZED FOR AT LEAST 72 HOURS PRIOR TO DIVERTING WATER FROM THE EXISTING AND/OR TEMPORARY CHANNEL.
- (3) INSTREAM EPSC DEVICES REQUIRE THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN WATER QUALITY PERMITS.
- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (5) THE WIDTH OF THE FILL ASSOCIATED WITH TEMPORARY CROSSINGS SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR THE ACTUAL CROSSING, NOT TO EXCEED THE WIDTH SPECIFIED IN THE STANDARD DRAWING.
- (6) STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CULVERT CROSSINGS SHALL BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES SHALL BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK SHALL BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS SHALL BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO PREEXISTING ELEVATIONS. ALL TEMPORARY CROSSINGS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (E.G. BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.
- (7) HEAVY EQUIPMENT WORKING IN WETLANDS WITH PERMITTED TEMPORARY IMPACTS SHALL BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE AND COMPACTION UNLESS SPECIFICALLY ADDRESSED IN THE CONSTRUCTION PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT SHALL BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED. ALL AFFECTED AREAS SHOULD BE RETURNED TO PRE-EXISTING CONDITIONS.
- (8) WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS. UNLESS SPECIFICALLY PROVIDED FOR IN THE CONSTRUCTION PLANS AND PERMITS.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

### SPECIES

- (10) NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA.

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

### PERMITS, PLANS & RECORDS

- (13) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY MATERIAL AND STAGING AREAS AND THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREA(S).
- (14) ANY DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT RESPONSIBLE PARTY. THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (16) THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATE. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.
- (17) ALL WATER QUALITY PERMITS SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.

### SUPPORT ACTIVITIES

- (18) MATERIALS AND STAGING AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY ENVIRONMENTAL PERMITS, OBTAINED SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATES. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

## ENVIRONMENTAL SPECIAL NOTES

### ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

### ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRECONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

### PROJECT COMMITMENTS

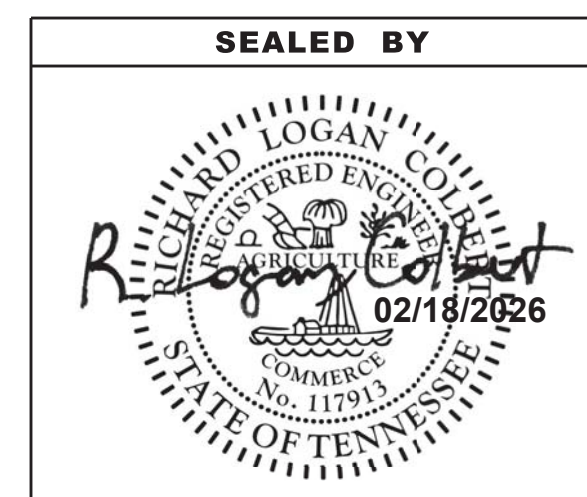
- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

### SCOPE OF WORK

- (6) SEE SHEET BR-133-330 FOR PROJECT SCOPE OF WORK.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	2D
PS&E	2026	181040-M3-010	2E

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL  
NOTES

# EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING, OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL

## DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.
- (2) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.
- (3) UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DISTURB ANY AREA BEYOND 15 FEET FROM SLOPE LINES.
- (4) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 14 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS APPLIED.
- (5) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

## SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (7) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY BUT MUST BE REINSTALLED AT THE END OF THE WORKDAY OR BEFORE/DURING A PRECIPITATION EVENT.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
- (9) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION EXIT (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.

- (10) THE DEWATERING OF WORK AREAS, TRENCHES, FOUNDATIONS, EXCAVATIONS, ETC. THAT HAVE COLLECTED STORMWATER, WATER FROM VEHICLE WASH AREAS, OR GROUNDWATER SHALL BE EITHER HELD IN SETTLING BASINS OR TREATED BY FILTRATION AND/OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER DISCHARGED SHALL NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITHIN THE RECEIVING NATURAL RESOURCE. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.

## INSPECTION, MAINTENANCE & REPAIR

- (12) THE TDOT CONSTRUCTION SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S RESPONSIBLE PARTY ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT CONSTRUCTION SUPERVISOR OR THEIR DESIGNEE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.
- (13) TDOT CONSULTANTS AND CONTRACTOR STAFF RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. TDOT STAFF AND SUPERVISORS RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDOT "FUNDAMENTALS OF EROSION AND SEDIMENT CONTROL" CLASS AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION.
- (14) EPSC CONTROLS SHALL BE INSPECTED ACCORDING TO PERMIT REQUIREMENTS TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE TDOT EPSC INSPECTION REPORT.
- (15) DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE ROADWAY SEDIMENT TRACKING.
- (16) UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24 HOUR TIMEFRAME, WRITTEN DOCUMENTATION SHALL BE PROVIDED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- (17) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES SHALL BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.
- (18) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.

- (19) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 209.03.02, FILTER SOCK (8 INCH), L.F.

## EROSION PREVENTION

- (20) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (21) THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CULVERT OR BRIDGE CONSTRUCTION, CUTTING, FILLING, OR ANY OTHER EARTHWORK OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (22) NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE TDOT RESPONSIBLE PARTY. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN.
- (23) TEMPORARY STABILIZATION SHALL BE INITIATED WITHIN 14 CALENDAR DAYS WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 CALENDAR DAYS. PERMANENT STABILIZATION MEASURES IN DISTURBED AREAS SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OF ANY PHASE OF CONSTRUCTION.
- (24) STEEP SLOPES SHALL BE TEMPORARILY STABILIZED NOT LATER THAN 7 DAYS AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED. STEEP SLOPES ARE DEFINED AS A NATURAL OR CREATED SLOPE OF 35% GRADE OR GREATER REGARDLESS OF HEIGHT.
- (25) PERMANENT STABILIZATION WILL REPLACE TEMPORARY MEASURES AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- (26) TEMPORARY OR PERMANENT STABILIZATION MUST BE FREE OF FINES (SILT AND CLAY SIZED PARTICLES). UNPACKED GRAVEL CONTAINING FINES OR CRUSHER-RUN WILL NOT BE CONSIDERED SUFFICIENT STABILIZATION.
- (27) DELAYING THE PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED.

## PERMITS, PLANS & RECORDS

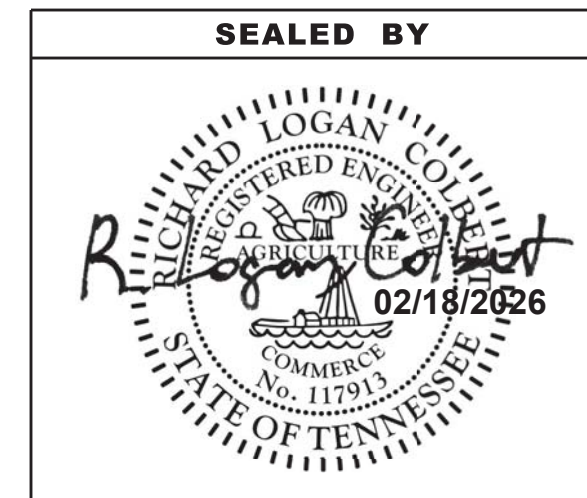
- (28) THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE STAGES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION, THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS PHASES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR; THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

## GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	3
PS&E	2026	181040-M3-010	2E1

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**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

**EPSC PLAN  
AND NOTES**

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# EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES (CONT.)

- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

## SUPPORT ACTIVITIES

- (41) IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.
- (42) MATERIALS AND STAGING AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN.
- (43) IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY EPSC PLANS FOR THE MATERIAL AND STAGING AREAS TO THE ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE FOR REVIEW.

## SPILL PREVENTION, MANAGEMENT & NOTIFICATION

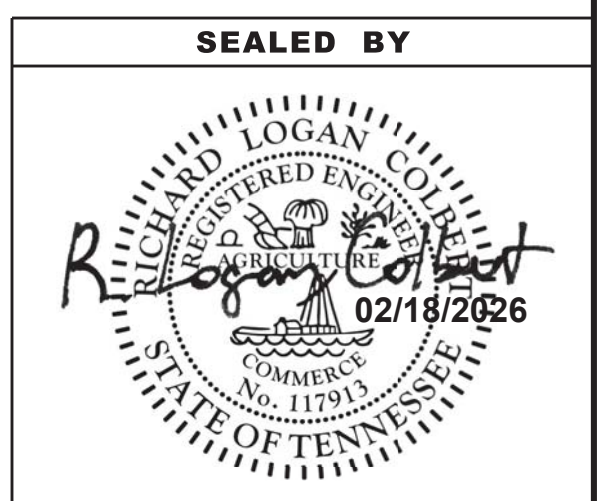
- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

# EROSION PREVENTION AND SEDIMENT CONTROL SPECIAL NOTES

## STREAMS, WETLANDS & BUFFER ZONES

- (1) FOR PROJECTS THAT DISCHARGE INTO KNOWN EXCEPTIONAL TENNESSEE WATERS OR WATERS IMPAIRED BY SILTATION, A 60 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM WITH THIS DESIGNATION SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 60 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 30 FEET AT ANY MEASURED LOCATION.
- (2) A 30 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 30 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 15 FEET AT ANY MEASURED LOCATION. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES.
- (3) BUFFER ZONES ARE NOT SEDIMENT CONTROL MEASURES AND MUST NOT BE RELIED UPON AS PRIMARY SEDIMENT CONTROL MEASURES. THE RIPARIAN BUFFER ZONE SHALL BE ESTABLISHED BETWEEN THE TOP OF THE STREAM BANK AND THE DISTURBED CONSTRUCTION AREA. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES. BEST MANAGEMENT PRACTICES (BMPs) PROVIDING EQUIVALENT PROTECTION AS THE NATURAL RIPARIAN ZONE MAY BE USED. WHERE ISSUED, ARAP/401 REQUIREMENTS WILL PREVAIL IF IN CONFLICT WITH THESE BUFFER ZONE REQUIREMENTS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	3A
PS&E	2026	181040-M3-010	2E2



**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

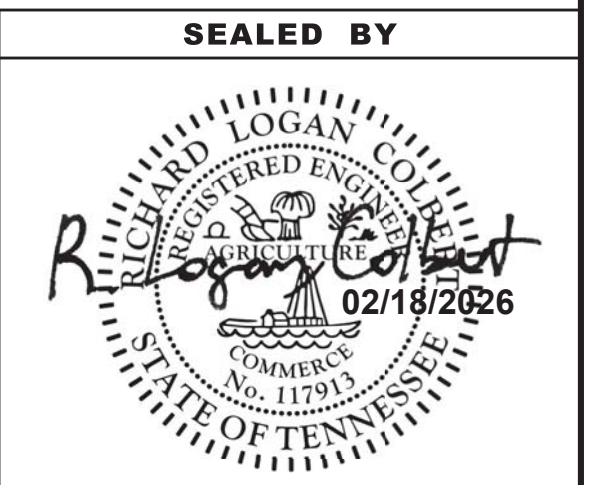
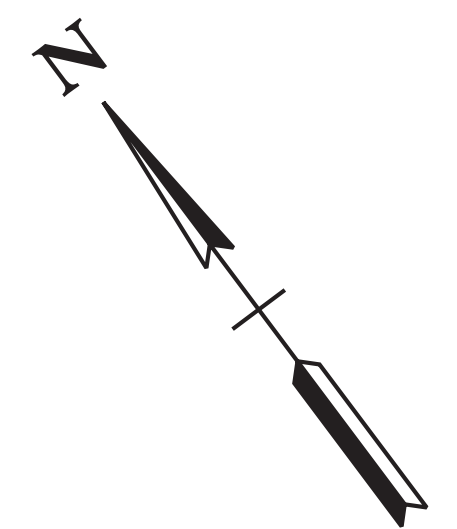
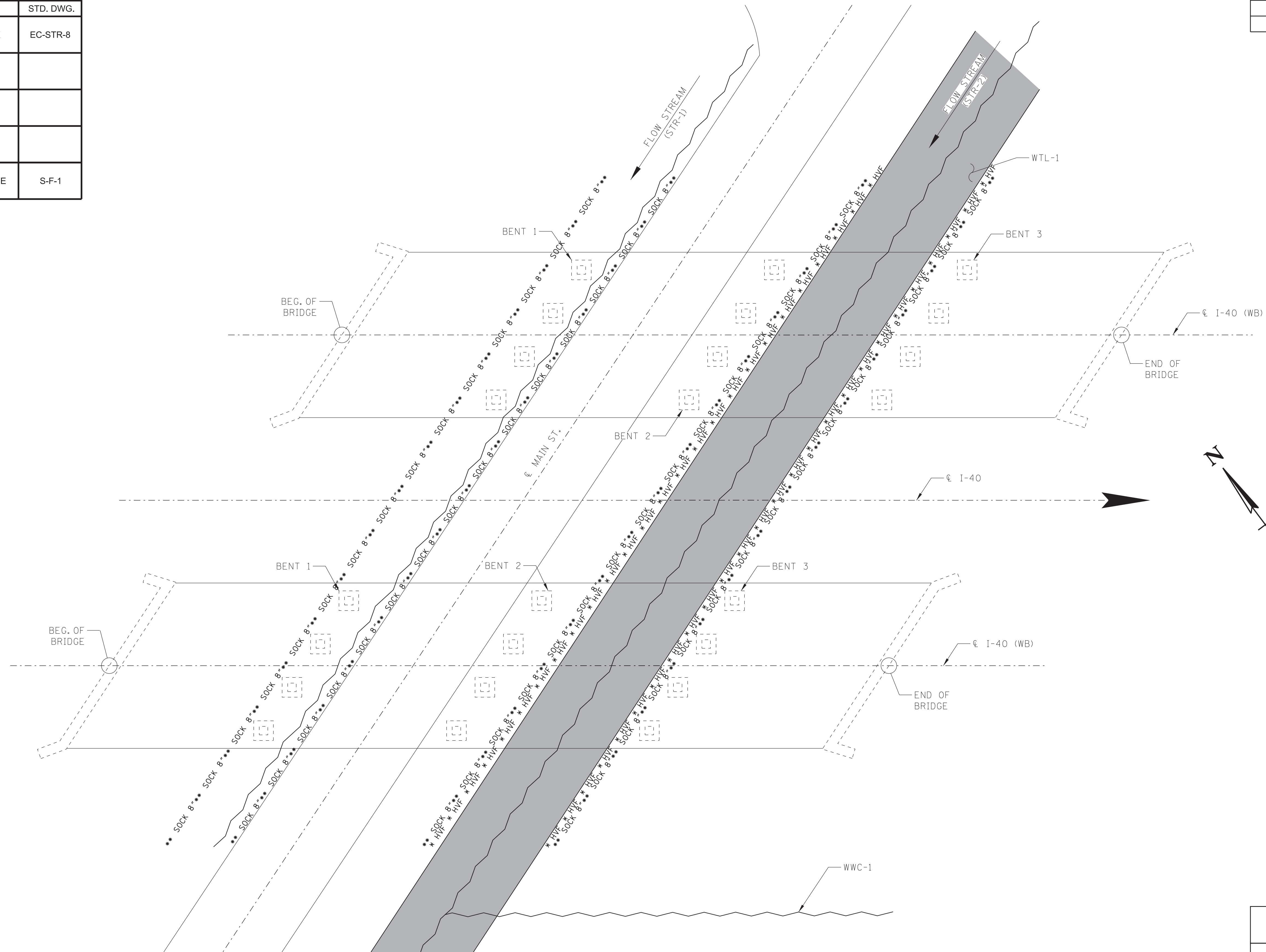
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AND NOTES**

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**EROSION PREVENTION AND  
 SEDIMENT CONTROL LEGEND**

SYMBOL	ITEM	STD. DWG.
** SOCK 8" **	8 INCH FILTER SOCK	EC-STR-8
STR	STREAM	
WWC	WET WEATHER CONVEYANCE	
WTL	WETLAND	
* HVF * HVF * HVF	HIGH VISIBILITY FENCE	S-F-1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	3B
PS&E	2026	181040-M3-010	2E3



**STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION**

**EPSC PLAN  
 AND NOTES**

# UTILITY NOTES

- (1) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER "THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT", THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

## UTILITY OWNERS

### CABLE:

**COMCAST (XFINITY)**  
 5720 ASHEVILLE HIGHWAY  
 KNOXVILLE, TN 37924  
 CONTACT: MR. JOSHUA JONES  
 OFFICE PHONE: 865 719 7590  
 CELL PHONE: N/A  
 Email: JOSHUA\_JONES@COMCAST.COM

### CABLE/FIBER:

**CHARTER COMMUNICATIONS**  
 851 S. WILLOW AVENUE, SUITE 206  
 COOKEVILLE, TN 38501  
 CONTACT: MR. MORGAN WILCHER  
 OFFICE PHONE: N/A  
 CELL PHONE: 931 239 9222  
 Email: MORGAN.WILCHER@CHARTER.COM

### ELECTRIC:

**VOLUNTEER ENERGY COOPERATIVE**  
 PO BOX 277  
 DECATUR, TN 37322  
 CONTACT: PRESIDENT MATTHEW TEAGUE  
 OFFICE PHONE: 423 334 7040  
 FAX: 423 334 7005  
 Email: MTEAGUE@VEC.ORG

### FIBER:

**BEN LOMAND CONNECT**  
 PO BOX 670  
 MCMINNVILLE, TN 37111  
 CONTACT: MR. RICHARD BOYD  
 OFFICE PHONE: 931 668 6692  
 CELL PHONE: 931 235 7515  
 Email: RICHARDBOYD@BENLOMAND.ORG

### FIBER:

**FRONTIER/CITIZENS COMMUNICATIONS COMPANY**  
 2104 WEST EMORY ROAD  
 POWELL, TN 37849  
 CONTACT: MR. JIM HEATHERLY  
 OFFICE PHONE: 865 947 8260  
 CELL PHONE: 865 236 5083  
 Email: JAMES.HEATHERLY@FTR.COM

### FIBER:

**VOLFIRST**  
 PO BOX 670  
 MCMINNVILLE, TN 37111  
 CONTACT: MR. RICHARD BOYD  
 OFFICE PHONE: 931 668 6692  
 CELL PHONE: 931 235 7515  
 Email: RICHARDBOYD@BENLOMAND.ORG

### FIBER:

**VOLUNTEER ENERGY COOPERATIVE**  
 PO BOX 277  
 DECATUR, TN 37322  
 CONTACT: PRESIDENT MATTHEW TEAGUE  
 OFFICE PHONE: 423 334 7040  
 FAX: 423 334 7005  
 Email: MTEAGUE@VEC.ORG

### GAS:

**MIDDLE TENNESSEE NATURAL GAS**  
 PO BOX 670  
 SMITHVILLE, TN 37166  
 CONTACT: MR. MATT STENNETT  
 OFFICE PHONE: 931 754 3515  
 CELL PHONE: 931 239 9111  
 Email: MSTENNETT@MTNG.COM

### SEWER:

**CITY OF CROSSVILLE**  
 392 NORTH MAIN STREET  
 CROSSVILLE, TN 38555  
 CONTACT: MR. TIM BEGLEY  
 OFFICE PHONE: 931 456 6172  
 CELL PHONE: 931 248 5172  
 Email: TIM.BEGLEY@CROSSVILLETN.GOV

### TELEPHONE

**BEN LOMAND CONNECT**  
 PO BOX 670  
 MCMINNVILLE, TN 37111  
 CONTACT: MR. RICHARD BOYD  
 OFFICE PHONE: 931 668 6692  
 CELL PHONE: 931 235 7515  
 Email: RICHARDBOYD@BENLOMAND.ORG

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 CONTACT: MR. JIM HEATHERLY  
 OFFICE PHONE: 865 947 8260  
 CELL PHONE: 865 236 5083  
 Email: JAMES.HEATHERLY@FTR.COM

### WATER:

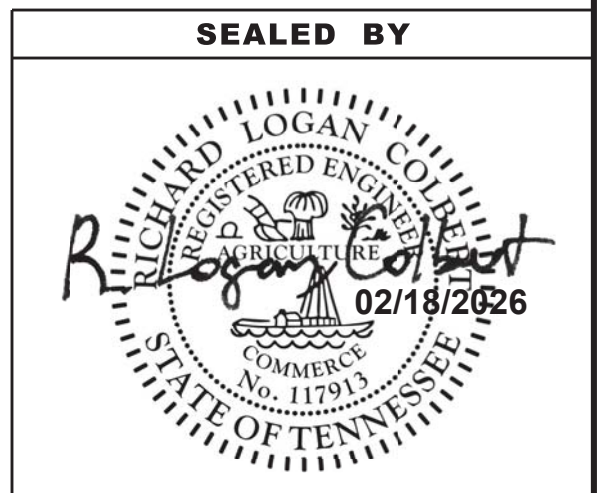
**CITY OF CROSSVILLE**  
 392 NORTH MAIN STREET  
 CROSSVILLE, TN 38555  
 CONTACT: MR. TIM BEGLEY  
 OFFICE PHONE: 931 456 5680  
 CELL PHONE: 931 260 7096  
 Email: TIM.BEGLEY@CROSSVILLETN.GOV

### WATER:

**CUMBERLAND PLATEAU WATER AUTHORITY**  
 2089 EAST 1<sup>ST</sup> STREET  
 CROSSVILLE, TN 38555  
 CONTACT: MR. JEFF DYER  
 OFFICE PHONE: 931 484 6987  
 FAX: 931 484 2970  
 Email: JEFF.DYER@CPWATN.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	3
PS&E	2026	181040-M3-010	3

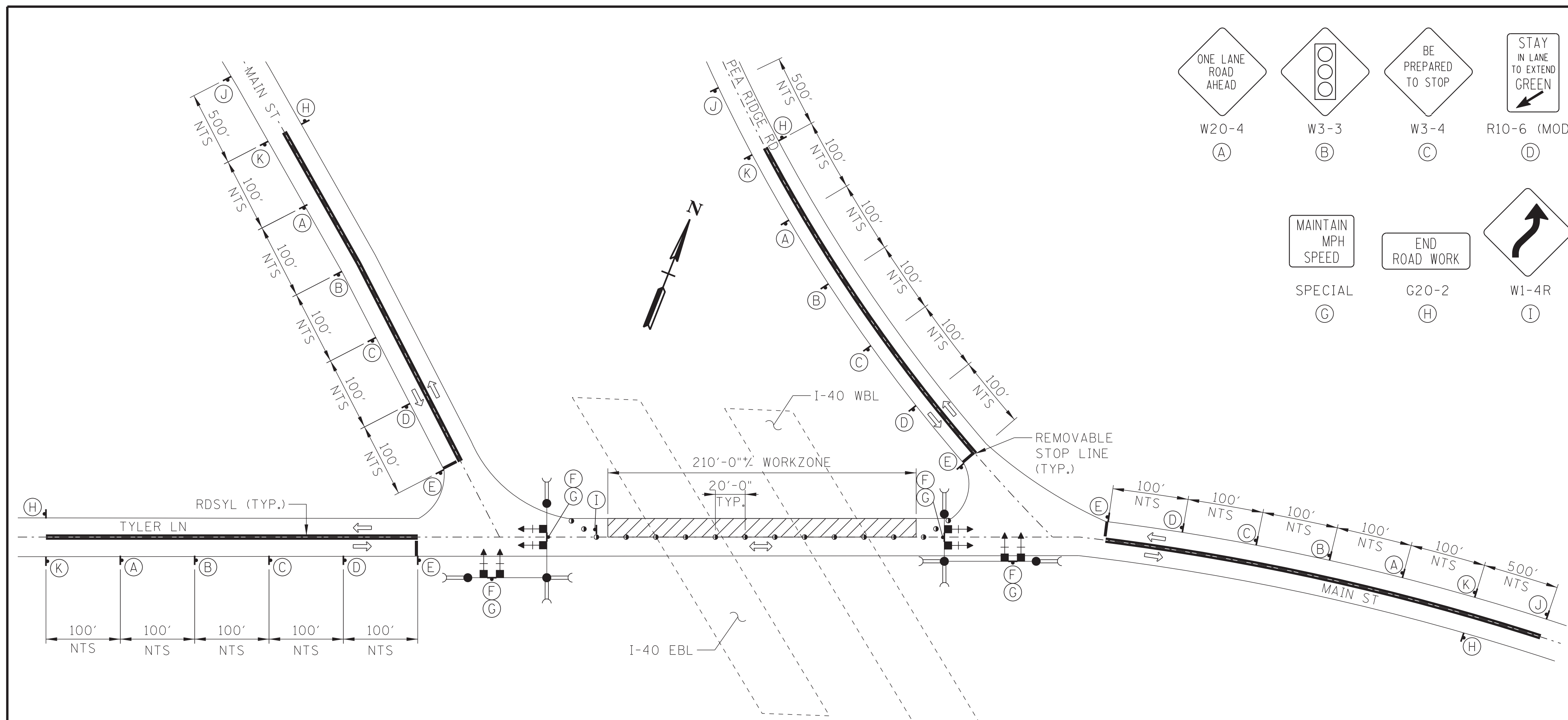
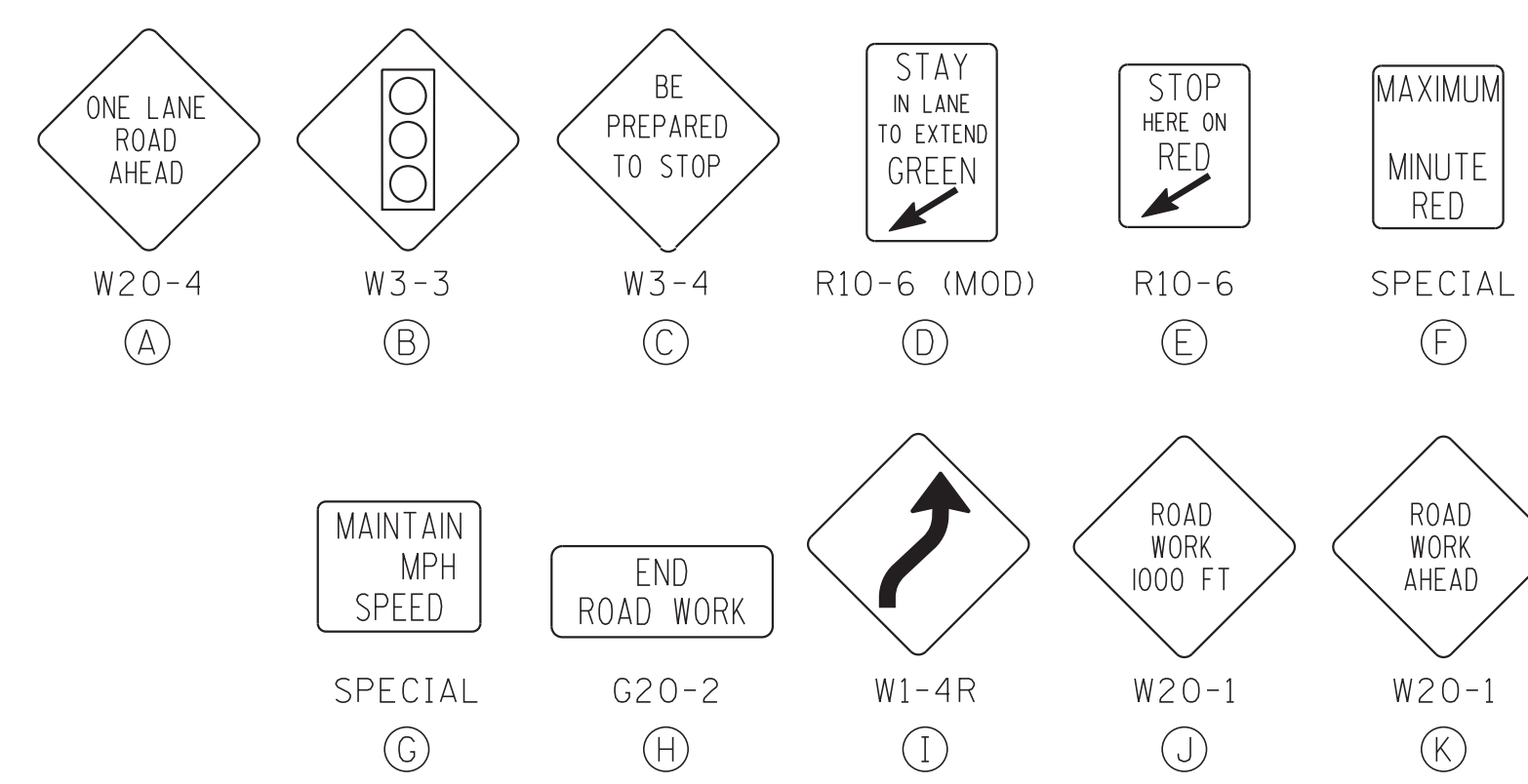
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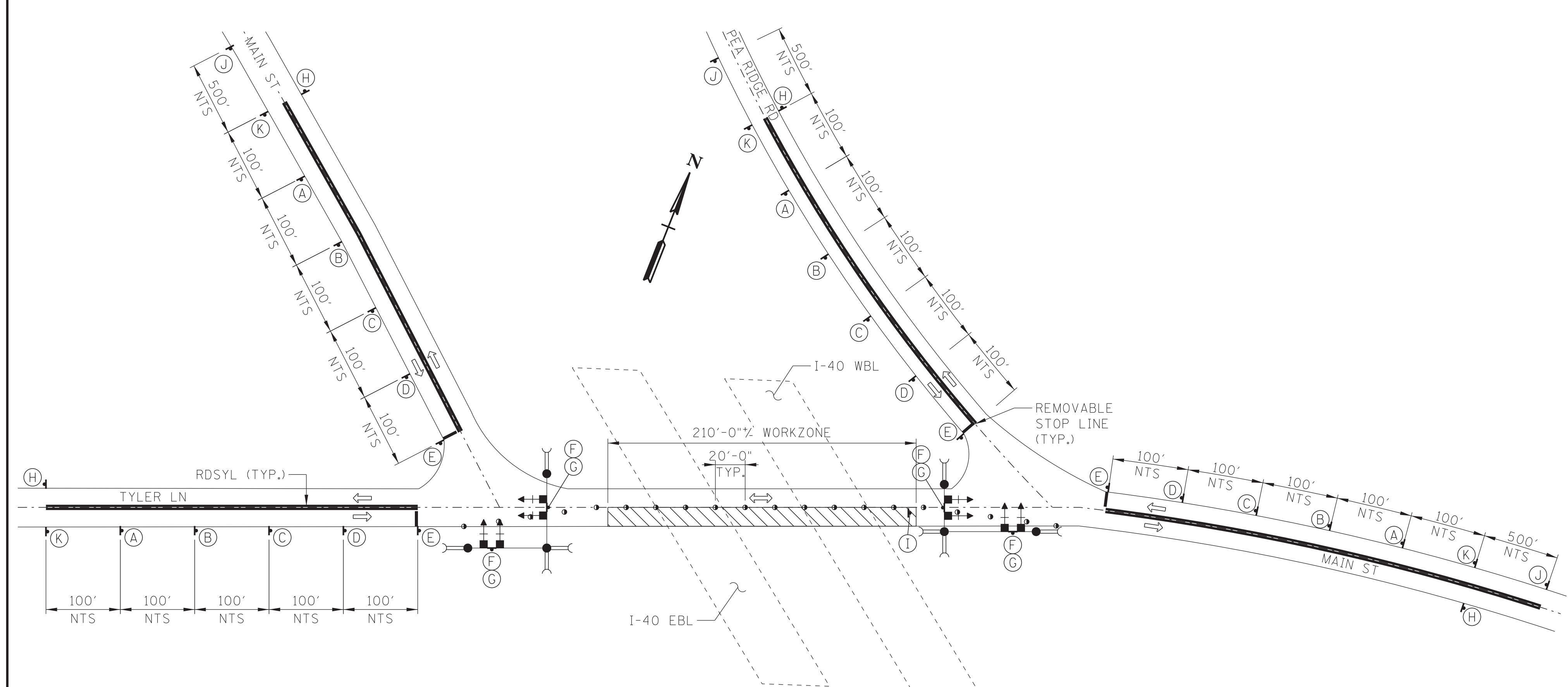
**STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION**

**UTILITY NOTES  
 AND  
 UTILITY OWNERS**

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2026	181040-M3-010	T1
PS&E	2026	181040-M3-010	T1



PHASE I CONSTRUCTION  
MAIN ST. BELOW BRIDGE

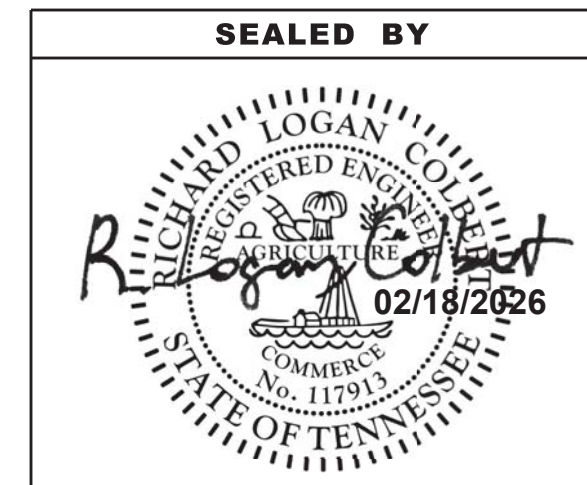


PHASE II CONSTRUCTION  
MAIN ST. BELOW BRIDGE

TRAFFIC CONTROL SPECIAL NOTES

1. THESE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES."
2. CONSTRUCTION SIGNING SHOWN IN THESE PLANS IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
3. THE CONTRACTOR IS REQUIRED TO PROVIDE LANE SHIFTS WHERE NECESSARY TO ROUTE TRAFFIC AROUND CONSTRUCTION.
4. NO TRAFFIC SHALL BE DETOURED OR ROADWAY CLOSED, ABANDONED, OR REMOVED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.
5. ADVANCE WARNING SIGNS ARE TO BE PLACED PRIOR TO BEGINNING OF CONSTRUCTION AND REMAIN IN PLACE UNTIL THE COMPLETION OF THIS PROJECT.
6. SEE THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" FOR TRAFFIC DETAILS NOT SHOWN, GENERAL TRAFFIC CONTROL NOTES, AND SIGN DETAILS.
7. CONTRACTOR TO COVER ALL CONFLICTING SIGNS DURING CONSTRUCTION. COVERINGS SHALL BE REMOVED AT COMPLETION OF CONSTRUCTION. COST ASSOCIATED WITH COVERING AND UNCOVERING SIGNS TO BE INCLUDED IN ITEM 712-06, SIGNS (CONSTRUCTION).
8. THE CONTRACTOR IS TO COORDINATE ALL LANE CLOSURES WITH THE TDOT REGIONAL OFFICE AND THE COUNTY ENGINEER.

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
•	FLEXIBLE DRUMS
	WORK ZONE
▸	SIGN
RDSYL	REMOVABLE DOUBLE SOLID YELLOW LINE
⇒	FLOW OF TRAFFIC
⊥	GUYING DEVICE VERTICAL ANCHOR (1/16" DIAMETER)
●	WOOD POLE
■	TEMPORARY TRAFFIC SIGNAL LIGHT



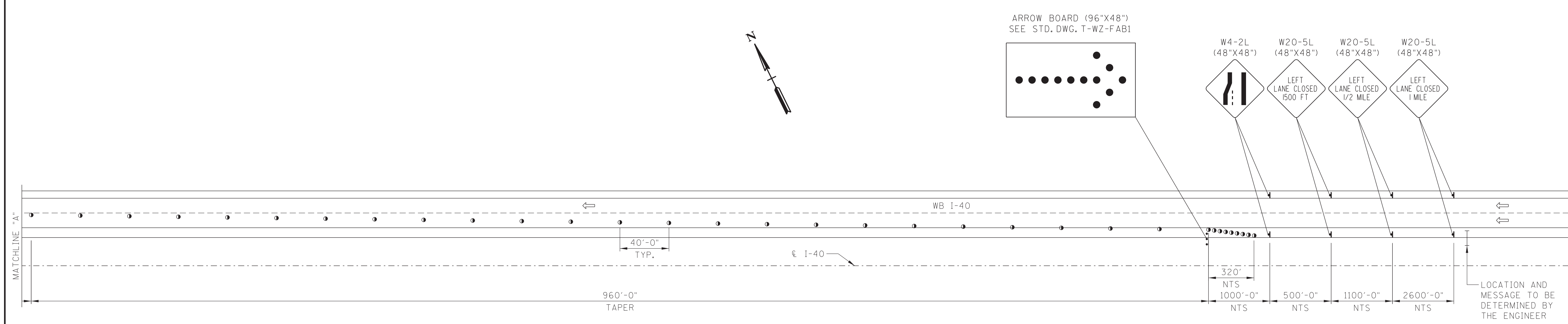
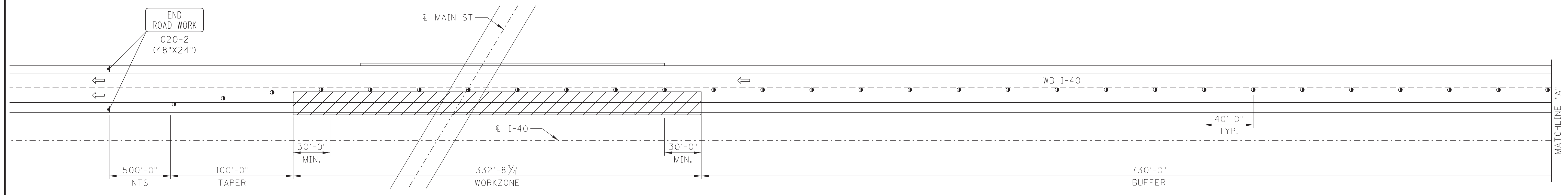
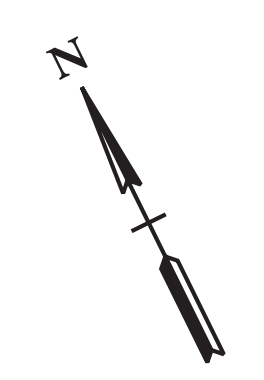
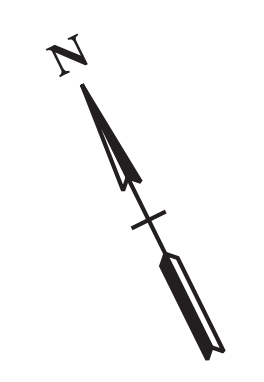
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL  
PLANS AND NOTES

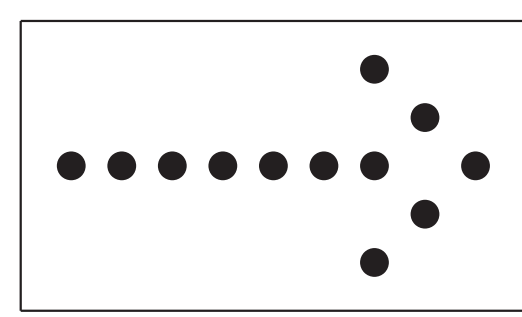
MAIN ST.

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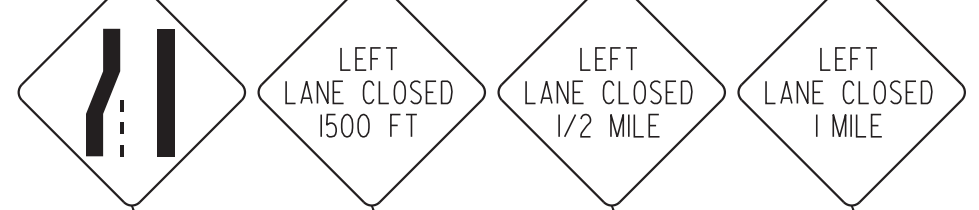
TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2026	181040-M3-010	T2



ARROW BOARD (96"X48")  
SEE STD. DWG. T-WZ-FAB1



W4-2L (48"X48")  
W20-5L (48"X48")  
W20-5L (48"X48")  
W20-5L (48"X48")



I-40 WESTBOUND LANE CLOSURE

**SEALED BY**

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

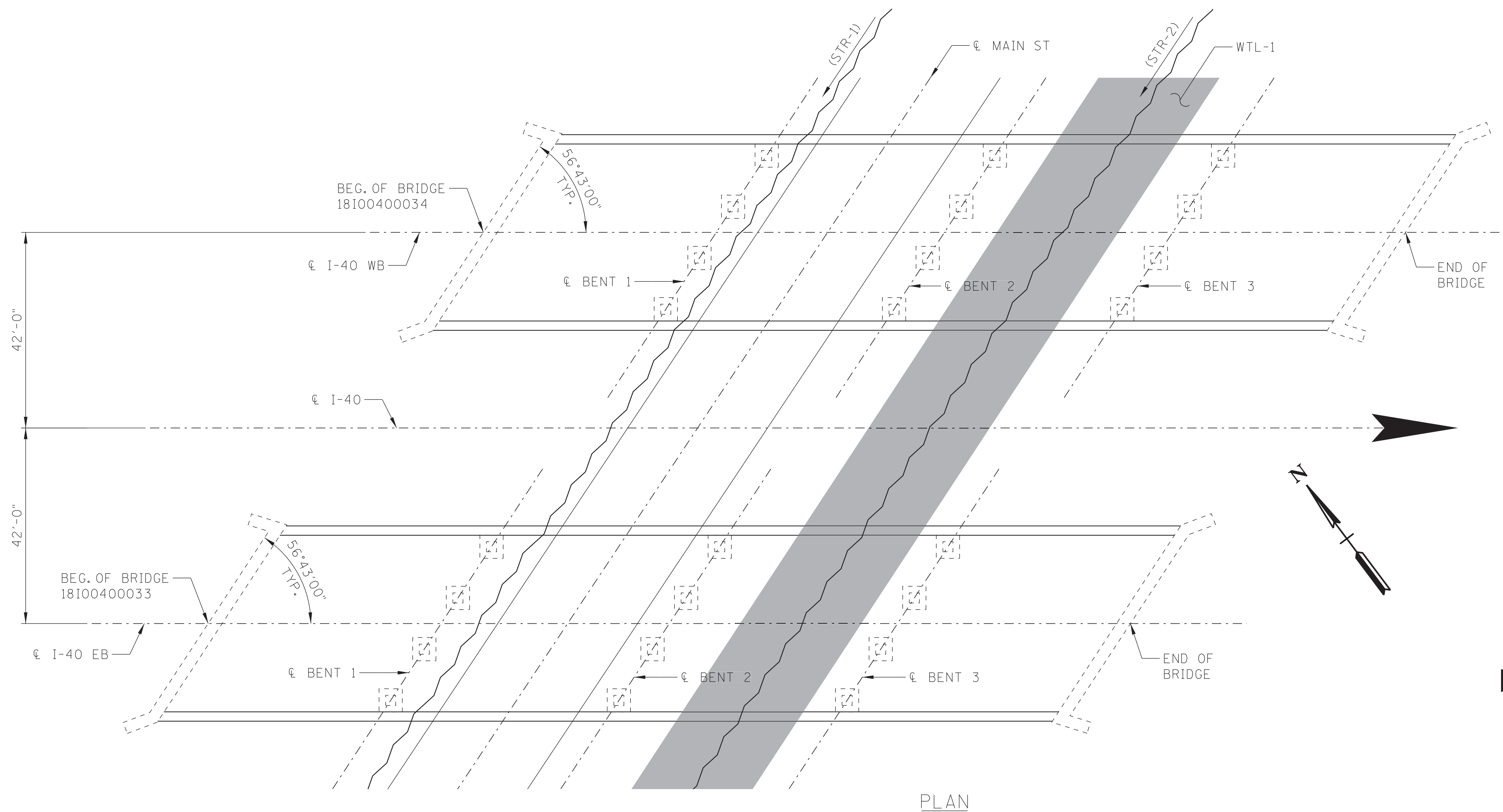
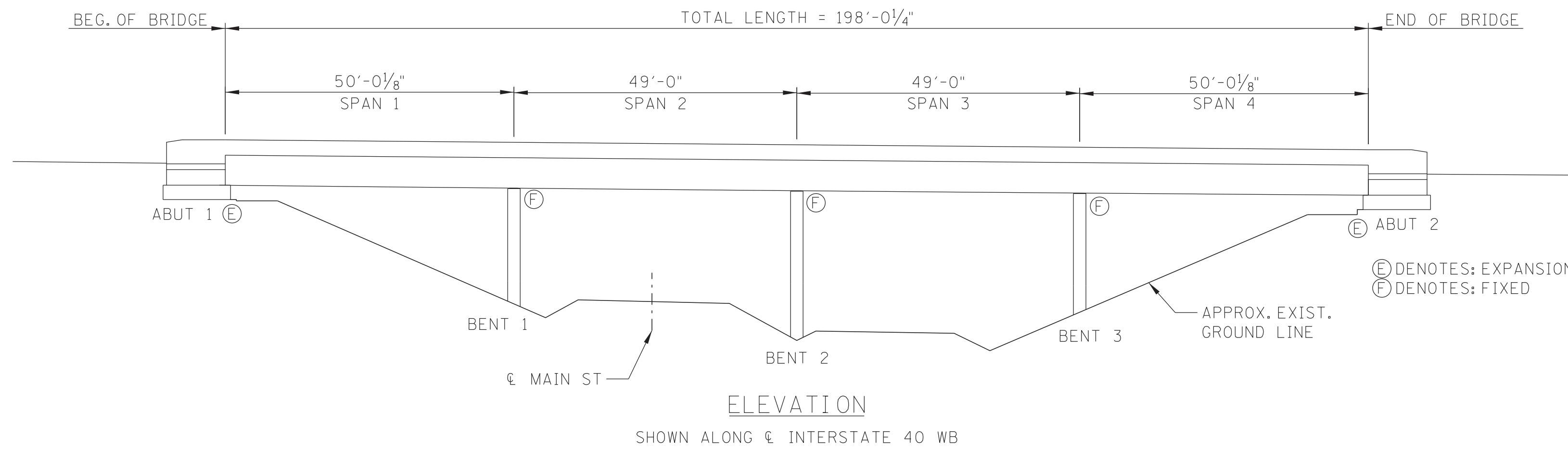
TRAFFIC CONTROL  
PLANS AND NOTES

INTERSTATE 40

TRAFFIC CONTROL LEGEND			
SYMBOL	ITEM	SYMBOL	ITEM
•	FLEXIBLE DRUMS	•••	FLASHING YELLOW ARROW BOARD
▨	WORK ZONE	⇨	FLOW OF TRAFFIC
▮	SIGN	NTS	NOT TO SCALE
—	CHANGEABLE MESSAGE SIGN		

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ALL DIMENSIONS OBTAINED FROM EXISTING PLANS DATED 1965.



**SCOPE OF WORK:**

1. PROVIDE TRAFFIC CONTROL FOR MAIN STREET (BELOW BRIDGE) AND WESTBOUND I-40.
2. REPAIR DAMAGED/DETERIORATED AREAS OF CONCRETE ON BEAMS, DIAPHRAGMS, AND SUBSTRUCTURE UNITS.
3. EPOXY-INJECT CRACKS IN BEAMS, DIAPHRAGMS, AND SUBSTRUCTURE UNITS.
4. APPLY FIBER-REINFORCED POLYMER (FRP) COMPOSITE WRAP TO COLUMNS AS INDICATED.
5. REPAIR VOID UNDER CONCRETE SLOPE PAVEMENT.
6. ALL DEBRIS/TRASH SHALL BE REMOVED FROM SUBSTRUCTURES AND BELOW BRIDGES (COST TO BE INCLUDED IN ITEM 201-05.31).
7. REMOVE VEGETATION (INCLUDING TREES) FOR DISTANCE OF TEN FEET ALONG EACH SIDE OF THE BRIDGES AS DIRECTED BY THE ENGINEER.

CONST. NO.:

PROJECT NO.	YEAR	SHEET NO.	
181040-M3-010	2026	B1	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

**BRIDGE INDEX**

DRAWING	DRAWING NO.	LAST REV. DATE
BRIDGE INDEX & LAYOUT OF BRIDGES TO BE REPAIRED	B1 (BR-133-330)	
ESTIMATED BRIDGE QUANTITIES AND BRIDGE NOTES	B2 (BR-133-331)	
BEAM REPAIRS	B3 (BR-133-332)	
CONCRETE REPAIRS (WBL)	B4 (BR-133-333)	
CONCRETE REPAIRS (EBL)	B5 (BR-133-334)	
FRP REPAIR DETAILS	B6 (BR-133-335)	
CONCRETE REPAIR DETAILS	B7 (BR-133-336)	

**LIST OF BRIDGE REFERENCE DRAWINGS**

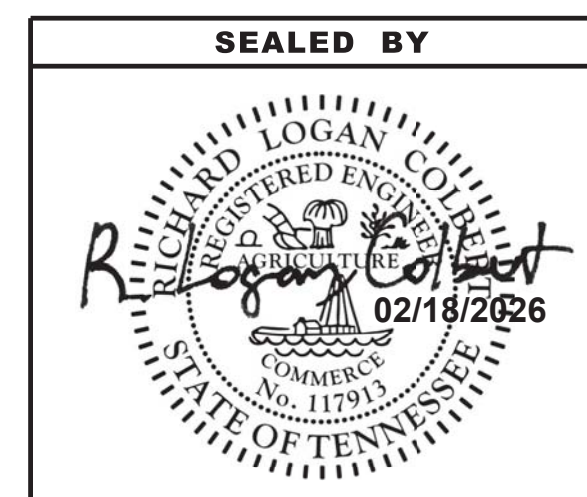
(TO BE PRINTED WITH PLANS)  
K-51-117 THRU K-51-122

**LIST OF SPECIAL PROVISIONS**

- SP604FRP - SPECIAL PROVISION REGARDING FIBER-REINFORCED POLYMER MATERIAL
- SP712PTQ - SPECIAL PROVISION REGARDING TRAFFIC QUEUE PROTECTION

I-40 POSTED SPEED LIMIT = 70 M.P.H.  
I-40 AADT (2023) = 34,465

THE CONTRACTOR SHALL USE ANY MEASURE NECESSARY TO ENSURE THAT NO CONSTRUCTION ACTIVITY WILL OCCUR IN, NOR THAT ANY CONSTRUCTION EQUIPMENT WILL ENTER ANY PORTION OF STR-1, STR-2, WWC-1, OR WTL-1 AND THAT THE STREAM AND SURROUNDING VEGETATION WILL NOT BE DISTURBED AND ARE PROTECTED FROM SEDIMENT AND OTHER POLLUTANTS EXCEPT AT THE PERMITTED LOCATIONS.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BRIDGE INDEX &  
LAYOUT OF BRIDGES TO BE REPAIRED  
I-40 OVER MAIN STREET  
BRIDGE NO. 18-10040-23.92 L&R  
FED. I.D. NO. 18100400033 & 18100400034  
CUMBERLAND COUNTY  
2026

BR-133-330

CONST. NO.:

PROJECT NO.	YEAR	SHEET NO.	
181040-M3-010	2026	B2	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

ESTIMATED BRIDGE QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	18100400034 (WB LANE)	18100400033 (EB LANE)	QUANTITY
(1)	201-05.31 VEGETATION REMOVAL	L.S.	0.5	0.5	1
(2)	604-10.05 CONCRETE	S.F.	53	40	93
(3)	604-10.24 JACKING CONCRETE SPANS	L.S.	0.5	0.5	1
(4)	604-10.42 CONCRETE REPAIRS	C.F.	5	4	9
(5)	604-10.54 CONCRETE REPAIRS	S.F.	53	40	93
	604-10.55 CONCRETE (FOUNDATION REPAIRS)	C.Y.	0	2	2
(6)	604-10.58 EPOXY INJECTION (INJECTION)	GAL.	7	4	11
(7)	604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)	L.F.	64	42	106
(8)	604-10.83 COMPOSITE FIBER ENCASEMENT	S.F.	78	52	130

FOOTNOTES	
(1)	INCLUDES COST OF ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY FOR THE REMOVAL AND DISPOSAL OF VEGETATION WITHIN 10 FEET OF THE STRUCTURE, AND ANY OTHER LOCATIONS NECESSARY TO COMPLETE THE WORK, AS DIRECTED BY THE ENGINEER. WHERE POSSIBLE, STUMPS AND ROOTS ARE TO REMAIN TO PREVENT GROUND DISTURBANCE. ITEM ALSO INCLUDES ALL COSTS ASSOCIATED WITH REMOVING TRASH AND DEBRIS FROM THE SUBSTRUCTURES AND BELOW THE BRIDGES.
(2)	INCLUDES COST OF ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PLACE HIGH EARLY STRENGTH CONCRETE FOR REPAIR AREAS AT LOCATIONS INDICATED IN PLANS.
(3)	INCLUDES ALL COSTS ASSOCIATED WITH JACKING AND SHORING OF SUPERSTRUCTURE TO ENABLE REPAIRS OF COLUMNS AND BEAMS AS INDICATED IN PLANS. ALSO INCLUDES ALL COSTS ASSOCIATED WITH THE COLUMN REPAIRS/STRENGTHENING AS SHOWN.
(4)	INCLUDES COST OF ALL LABOR, EQUIPMENT, AND MATERIALS REQUIRED FOR REPAIRING SPALLED OR DELAMINATED AREAS OF CONCRETE ON BEAMS AT LOCATIONS SHOWN IN THE PLANS.
(5)	INCLUDES COST OF ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PLACE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL FOR REPAIR AREAS AT LOCATIONS INDICATED IN PLANS.
(6)	INCLUDES ALL COSTS ASSOCIATED WITH PROVIDING EPOXY.
(7)	INCLUDES ALL COSTS ASSOCIATED WITH PROVIDING EPOXY INJECTION TO LOCATIONS SHOWN IN THE PLANS.
(8)	INCLUDES COST OF ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS REQUIRED TO PLACE FIBER REINFORCED POLYMER WRAP AT LOCATIONS INDICATED IN THE PLANS, IN ACCORDANCE WITH TDOT SPECIAL PROVISION 604FRP AND MANUFACTURER'S RECOMMENDATIONS.

**GENERAL NOTES**

**SPECIFICATIONS & LOADING**

- (1) **CONSTRUCTION SPECIFICATIONS:** STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (JANUARY 1, 2021 EDITION), AND THE 4<sup>TH</sup> EDITION (2017) AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS WITH INTERIMS.
- (2) **DESIGN SPECIFICATIONS:** 10<sup>TH</sup> EDITION (2024) AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS WITH INTERIMS, AND THE 2<sup>ND</sup> EDITION (2011) AASHTO GUIDE SPECIFICATIONS FOR LRFD SEISMIC BRIDGE DESIGN WITH INTERIMS.

**STEEL, CONCRETE, REINFORCING, AND FORMING**

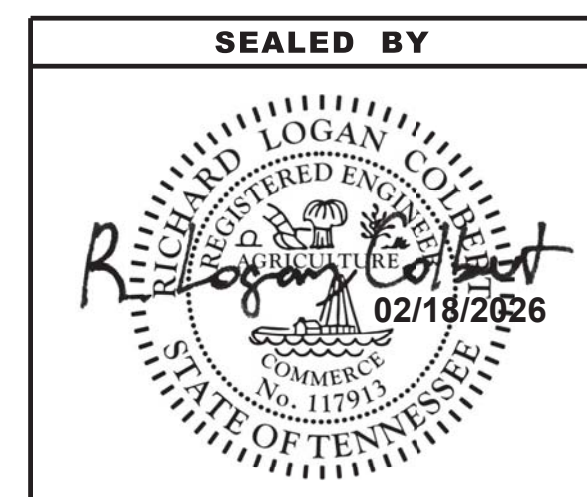
- (3) **REINFORCING STEEL:** SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.
- (4) **CONCRETE:** TO BE CLASS A (CAST-IN-PLACE) F'C = 3000 PSI EXCEPT AS NOTED OTHERWISE.
- (5) **CONCRETE CURING:** ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.

**MISCELLANEOUS GENERAL NOTES**

- (6) **SHOP DRAWINGS:** REFER TO SECTION 105.02 OF THE STANDARD SPECIFICATIONS. IF USING PAPER COPIES, SHOP DRAWINGS ARE TO BE SENT TO THE BRIDGE REPAIR OFFICE IN THE DIVISION OF STRUCTURES, FOR ELECTRONIC SUBMITTALS, SEE SECTION 105.02 OF THE STANDARD SPECIFICATIONS. EACH SHOP DRAWING SHALL CONTAIN IN THE TITLE BLOCK THE FOLLOWING: THE STATE PROJECT NUMBER, COUNTY, BRIDGE NAME, BRIDGE NUMBER (OR STRUCTURE TYPE AND NUMBER), STATION, AND CONTRACT NUMBER. SHOP DRAWINGS WITH TITLE BLOCKS NOT INCLUDING THE FOREGOING IDENTIFICATION WILL BE RETURNED FOR CORRECTION BEFORE ANY REVIEWS FOR APPROVAL ARE CONDUCTED.
- (7) **SPECIAL NOTE TO CONTRACTOR:** CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURES NECESSARY TO ENSURE THAT NO DEBRIS IS DROPPED INTO THE STREAM. ANY DEBRIS WHICH IS ALLOWED TO DROP ON THE BANKS BELOW THE BRIDGE SHALL NOT BE ALLOWED TO ENTER THE STREAM AND SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. COST OF REMOVAL AND DISPOSAL OF DEBRIS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR OTHER ITEMS.
- (8) **DEMOLITION:** THE CONTRACTOR SHALL TAKE SPECIAL CARE TO PROTECT ANY PARTS OF THE STRUCTURE THAT ARE NOT TO BE REMOVED SPECIFICALLY. FOR FULL DEPTH SLAB REMOVAL, EXCEPT OVER BEAMS, THE MAXIMUM HAMMER SIZE IS 90 POUND CLASS. FOR PARTIAL DEPTH SLAB REMOVAL AND ANY WORK OVER THE BEAMS, THE MAXIMUM HAMMER SIZE IS 60 POUND CLASS; CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL. SAWING OR CUTTING OF THE CONCRETE IS ACCEPTABLE AS LONG AS ANY SPECIFIED PROJECTION OF THE EXISTING REINFORCING STEEL IS MAINTAINED. EXPANSION JOINT REMOVAL SHALL FOLLOW THE SAME RESTRICTIONS AS FULL DEPTH SLAB REMOVAL. ALL DEVICES PROPOSED FOR CONCRETE DEMOLITION SHALL MEET THE APPROVAL OF THE ENGINEER.
- (9) THE CONTRACTOR IS NOT ALLOWED TO USE A HYDRAULIC RAM MOUNTED ON A BACKHOE (COMMONLY CALLED A HOE RAM), MINI EXCAVATOR, OR OTHER EQUIPMENT FOR ANY CONCRETE REMOVAL.
- (10) **QUICK-SET PATCHING MATERIAL:** QUICK-SET PATCHING MATERIAL SHALL BE A POLYMER MODIFIED CEMENTITIOUS PATCHING MATERIAL. SEE TDOT QUALIFIED PRODUCTS LIST 13.009 POLY MOD CEMENT STRUCT PATCH VERT & OVER FOR ACCEPTABLE PATCHING MATERIALS.

**SPECIAL NOTES**

- (11) **HIGH EARLY STRENGTH CONCRETE:** THE MIX IS TO MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS, CLASS X. THE CEMENT CONTENT SHALL BE A MINIMUM OF 714 LBS. THE WATER-CEMENT RATIO SHALL BE A MAXIMUM OF 0.40. DESIGN AIR CONTENT SHALL BE 6% WITH ±2% ACCEPTANCE RANGE IN THE FIELD. SLUMP SHALL BE 3±1 INCHES. IF USING A TYPE A, F, OR G WATER REDUCER, THE SLUMP SHALL BE MAXIMUM OF 8 INCHES. NO FLY ASH REPLACEMENT WILL BE PERMITTED. THE MINIMUM 28 DAY COMPRESSIVE STRENGTH SHALL BE 3,500 PSI. THE CONTRACTOR SHALL PROVIDE PROOF PRIOR TO BEGINNING WORK THAT THE PROPOSED CONCRETE MIX WILL OBTAIN THE REQUIRED PROPERTIES. PROOF SHALL BE PROVIDED BY AN INDEPENDENT TESTING COMPANY AND SUBMITTED TO THE MATERIALS AND TESTS DIVISION OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION FOR APPROVAL. TRAFFIC SHALL NOT BE PERMITTED ON ANY OF THE REPAIRED AREAS UNTIL TEST SPECIMENS ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF EIGHTEEN (18) HOURS.
- (12) **FORMS AND FALSEWORK:** ALL CONCRETE FORMS AND FALSEWORK SHALL BE REMOVED AFTER REPAIRS ARE COMPLETED. COST OF REMOVAL SHALL BE INCLUDED IN ITEMS BID ON. THIS WORK SHALL BE COMPLETED BEFORE FINAL PAYMENT IS APPROVED.
- (13) THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS AND CONSTRUCTION.
- (14) ANY AREA DISTURBED OUTSIDE THE LIMITS OF THE CONSTRUCTION DURING THE LIFE OF THE PROJECT SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ESTIMATED BRIDGE QUANTITIES  
AND BRIDGE NOTES  
I-40 OVER MAIN STREET  
BRIDGE NO. 18-I0040-23.92 L&R  
FED. I.D. NO. 18100400033 & 18100400034  
CUMBERLAND COUNTY  
2026

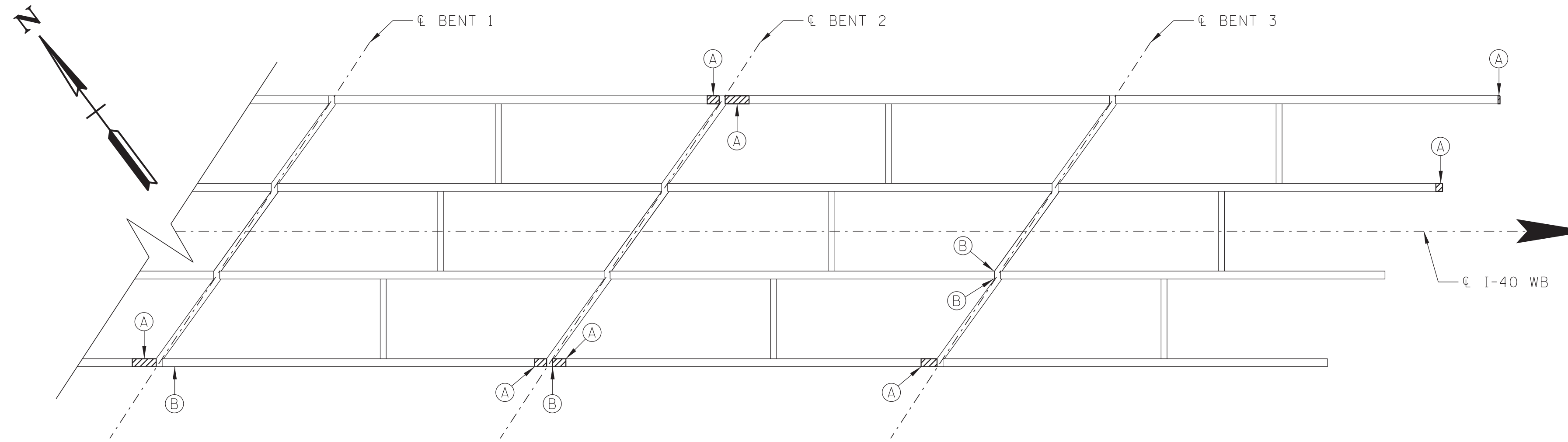
BR-133-331

2/18/2026 9:10:25 AM J:\Structures\12477-29\_Cumberland\_Co\_I-40\_over\_Main\_S\Drawings\Final\_dgn\12477-29\_STR\_EST QUANTITIES & NOTES.dgn

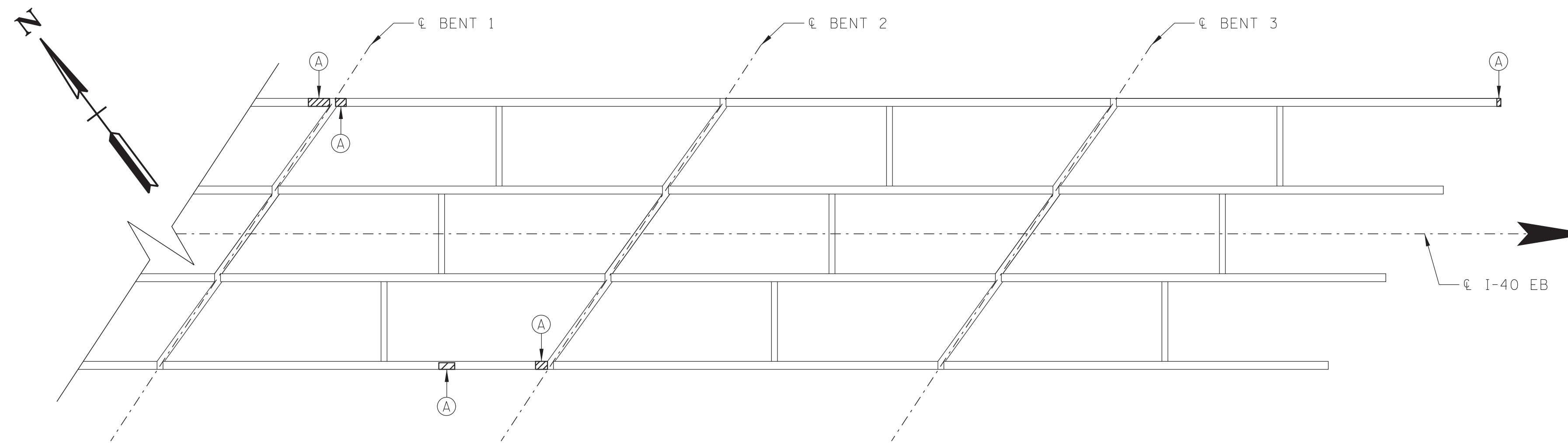
	PIN NO.: 136145.00	DATE: 02/2026
DESIGN BY: R.L. COLBERT	DATE: 02/2026	
DRAWN BY: M.E. LOUCKS	DATE: 02/2026	
SUPERVISED BY: G.S. WILSON	DATE: 02/2026	
CHECKED BY: R.L. COLBERT	DATE: 02/2026	

CONST. NO.:

PROJECT NO.	YEAR	SHEET NO.	
181040-M3-010	2026	B3	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



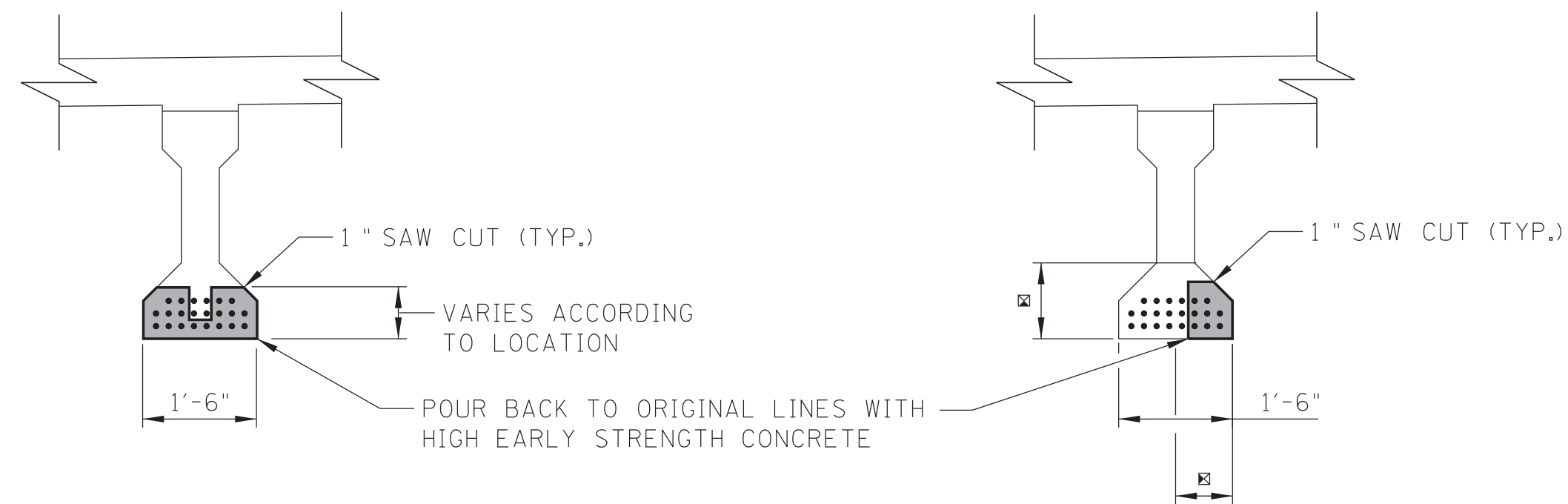
PARTIAL FRAMING PLAN (WBL)



PARTIAL FRAMING PLAN (EBL)

ESTIMATED QUANTITIES

LOCATION OF REPAIR	ITEM 604-10.42 APPROX. REPAIR AREAS (C.F.)	ITEM 604-10.62 APPROX. REPAIR AREAS (L.F.)
SPAN 1 (WBL)	1	-
SPAN 2 (WBL)	1	2
SPAN 3 (WBL)	2	3
SPAN 4 (WBL)	1	-
SPAN 1 (EBL)	1	-
SPAN 2 (EBL)	2	-
SPAN 3 (EBL)	-	-
SPAN 4 (EBL)	1	-
TOTAL	9	5



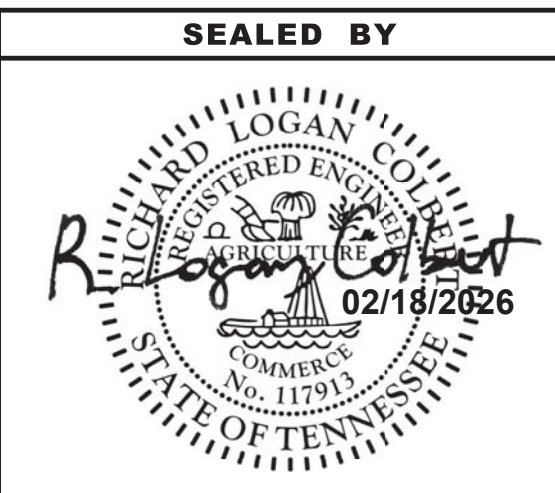
BEAM REPAIR DETAILS

THE COST OF REMOVING PORTIONS OF THE EXISTING CONCRETE WITHIN THE LIMITS SHOWN, SAW CUTTING, COMPLETELY CLEANING EXISTING REINFORCING STEEL, HIGH EARLY STRENGTH CONCRETE, FORMING, LABOR, AND ALL MISCELLANEOUS MATERIAL NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F.

CONTRACTOR TO TAKE EXTREME CARE WHEN REMOVING DELAMINATED CONCRETE SO AS NOT TO DAMAGE EXISTING REINFORCING STEEL. ALL EXISTING REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE REPOURING.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS TO THE BEAMS. DETAILS OF ANY TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE THE BEGINNING OF ANY BEAM REPAIR. COST TO BE INCLUDED IN ITEM NO. 604-10.24, JACKING CONCRETE SPANS, L.S.

- (A) DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.42. SEE BEAM REPAIR DETAILS ON THIS SHEET.
- (B) DENOTES CRACK TO BE REPAIRED UNDER ITEM NO. 604-10.58 AND ITEM NO. 604-10.62. SEE EPOXY INJECTION NOTES ON BR-133-336.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

BEAM REPAIRS  
I-40 OVER MAIN STREET  
BRIDGE NO. 18-I0040-23.92 L&R  
FED. I.D. NO. 18100400033 & 18100400034  
CUMBERLAND COUNTY  
2026

BR-133-332

CONST. NO.:

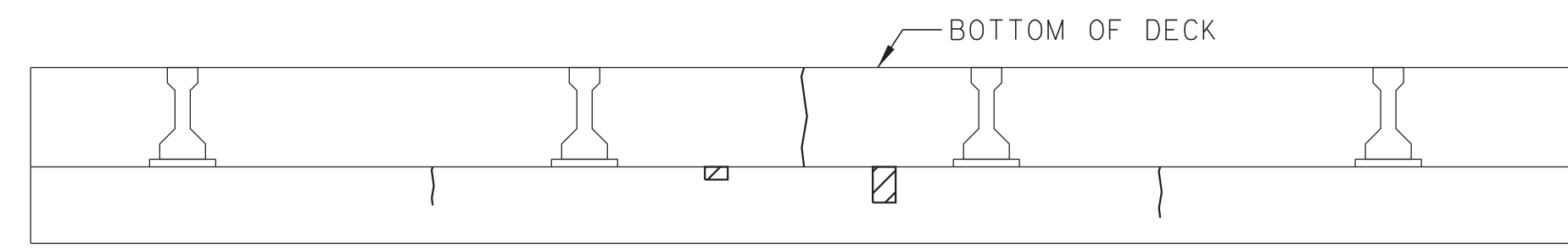
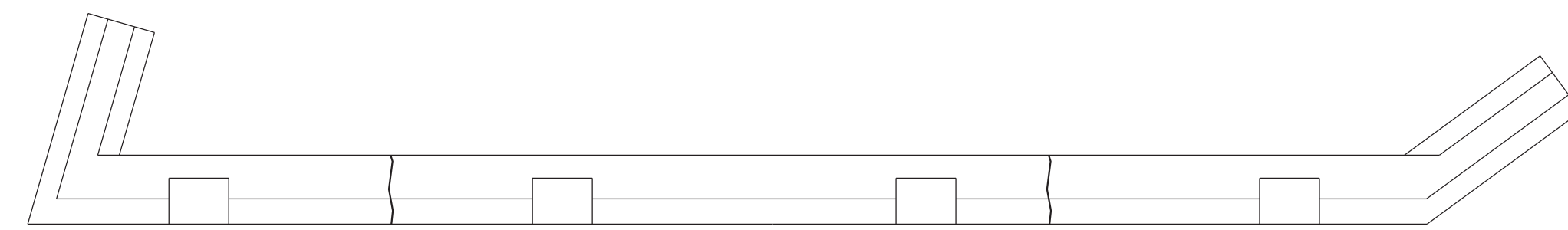
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181040-M3-010	2026	B4	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

ESTIMATED QUANTITIES

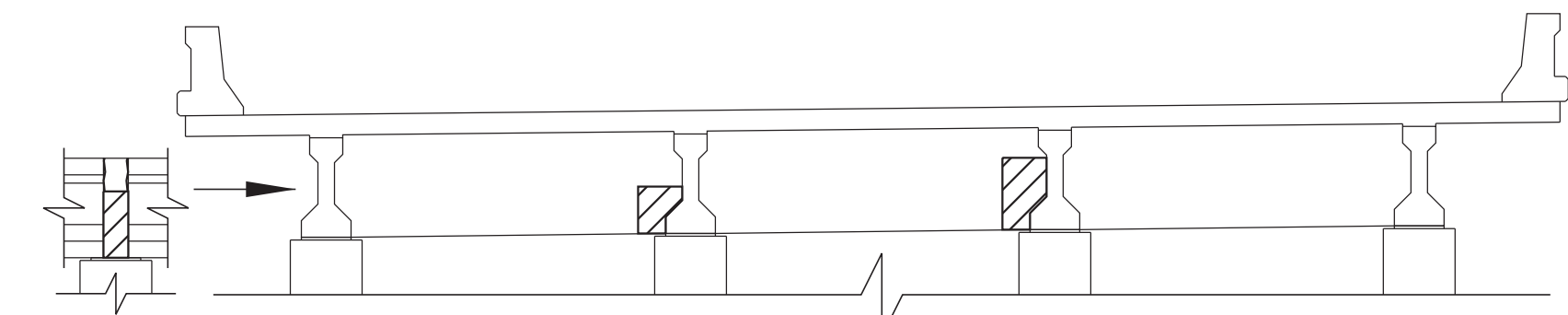
LOCATION OF REPAIR	ITEM 604-10.05 AND/OR 604-10.54 APPROX. REPAIR AREAS (S.F.)	ITEM 604-10.62 APPROX. REPAIR AREAS (L.F.)
ABUT. 1	2	12
BENT 1	30	17
BENT 2	43	-
BENT 3	27	-
ABUT. 2	4	30
TOTAL	106	59

 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. FOR DETAILS SHOWING AREAS OF EXISTING SPALLED OR DELAMINATED CONCRETE SURFACES TO BE REMOVED AND REPAIRED, SEE SHEET BR-133-336.

 DENOTES CRACK TO BE REPAIRED UNDER ITEM NO. 604-10.58 AND 604-10.62. SEE EPOXY INJECTION NOTES ON SHEET BR-133-336.

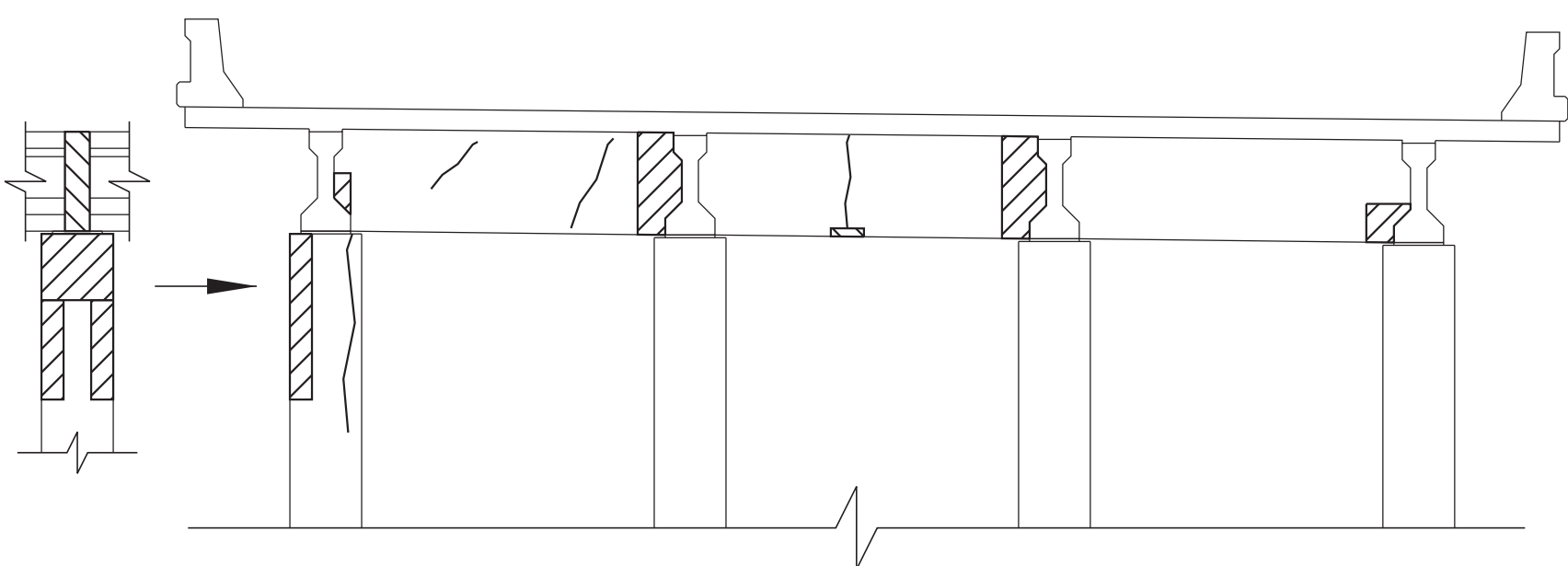


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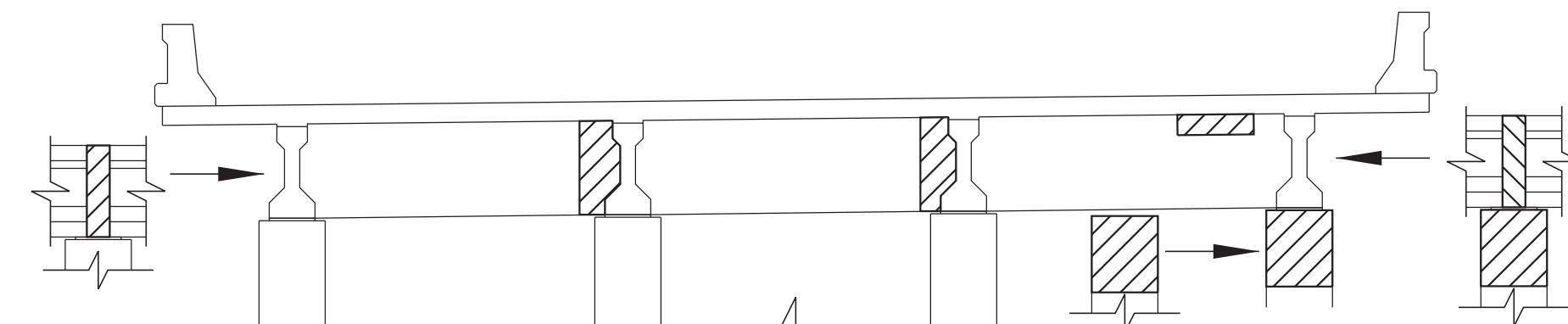


BENT 1  
LOOKING AHEAD

CONC. SLOPE PVMT.

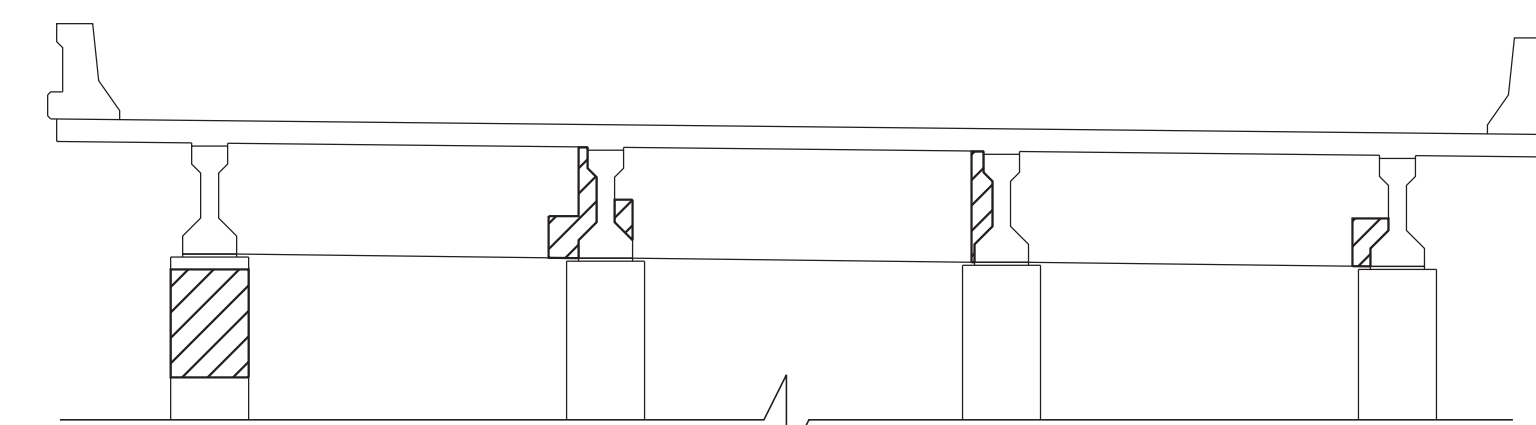


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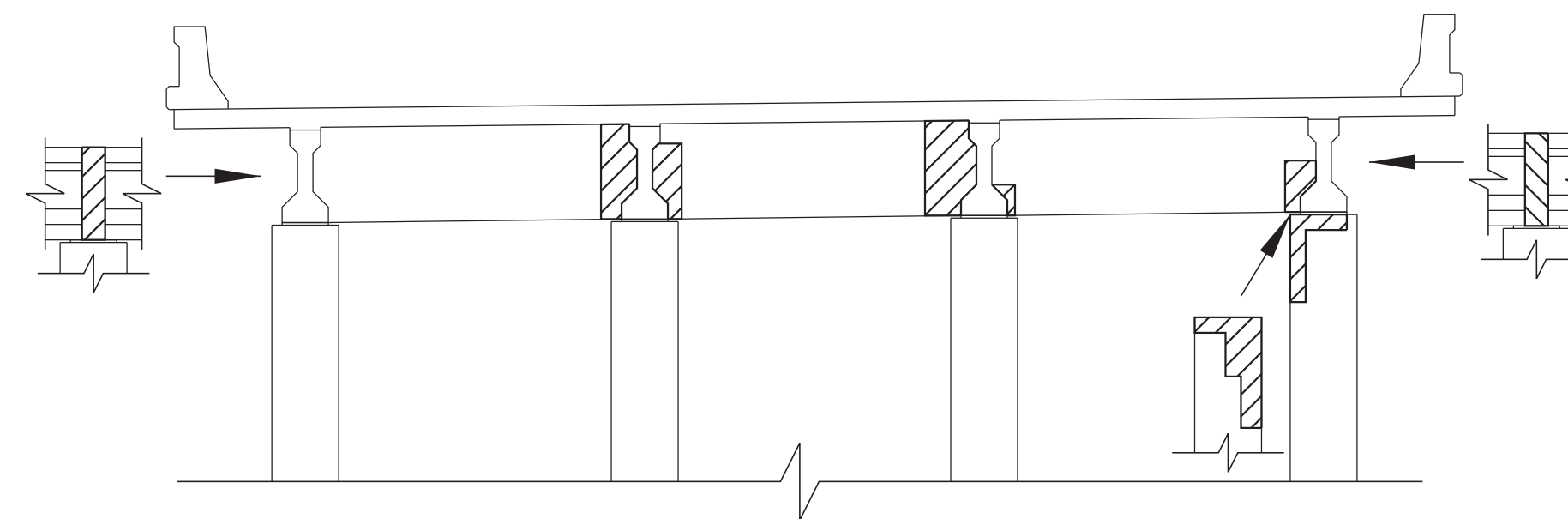
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GROUNDLINE

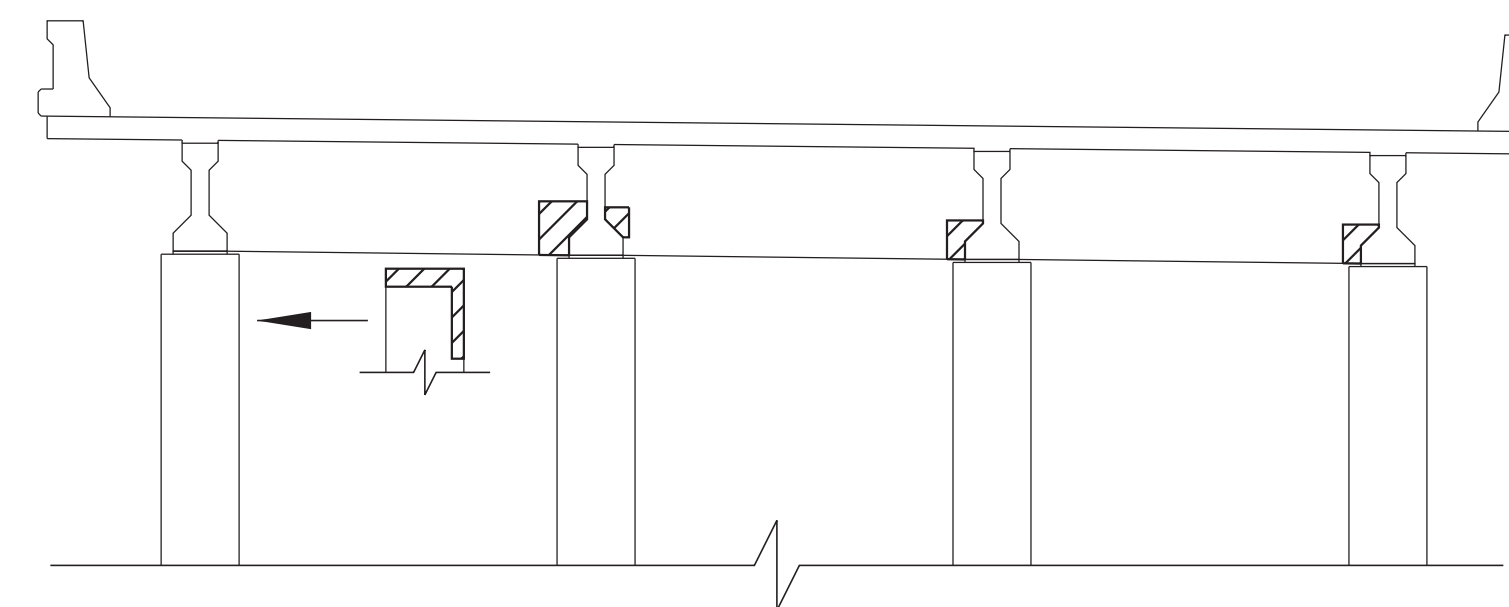


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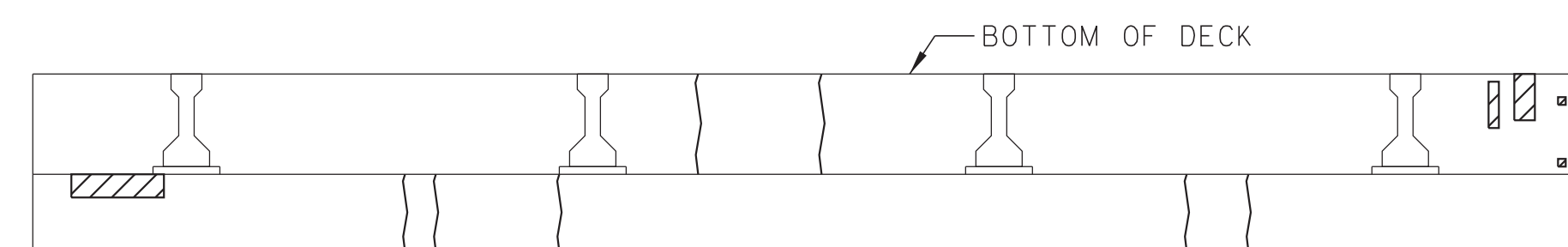
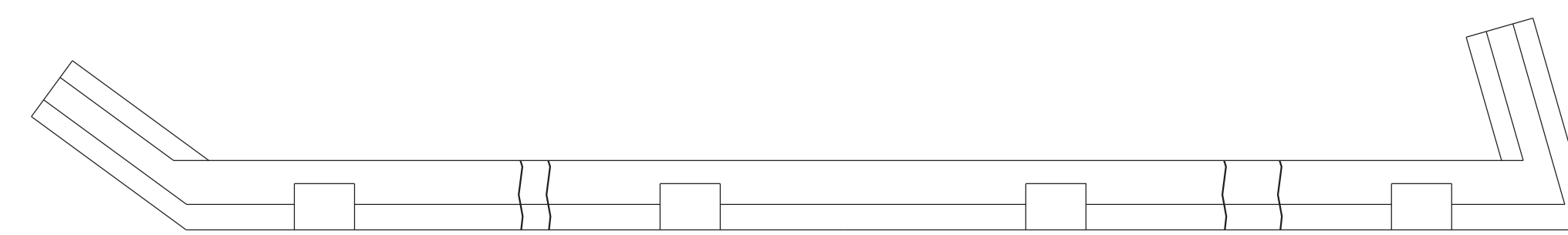
GROUNDLINE



BENT 3  
LOOKING AHEAD



BENT 3  
LOOKING BACK



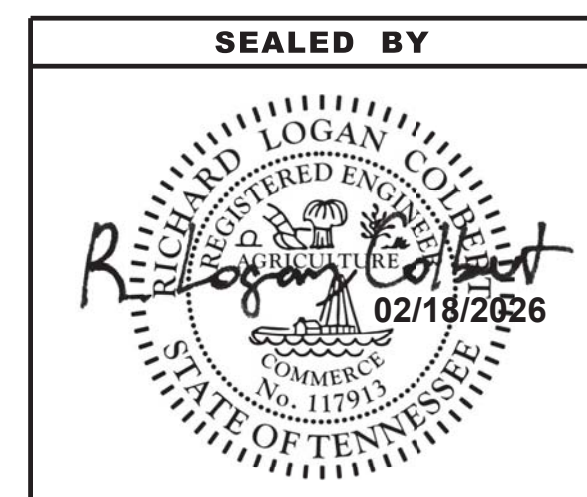
ABUTMENT 2  
LOOKING AHEAD

BOTTOM OF DECK

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PIN NO.: 136145.00  
 DESIGN BY: R.L. COLBERT DATE: 02/2026  
 DRAWN BY: M.E. LOUCKS DATE: 02/2026  
 SUPERVISED BY: G.S. WILSON DATE: 02/2026  
 CHECKED BY: R.L. COLBERT DATE: 02/2026



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIRS (WBL)  
I-40 OVER MAIN STREET  
BRIDGE NO. 18-I0040-23.92 L&R  
FED. I.D. NO. 18100400033 & 18100400034  
CUMBERLAND COUNTY  
2026

BR-133-333

CONST. NO.:

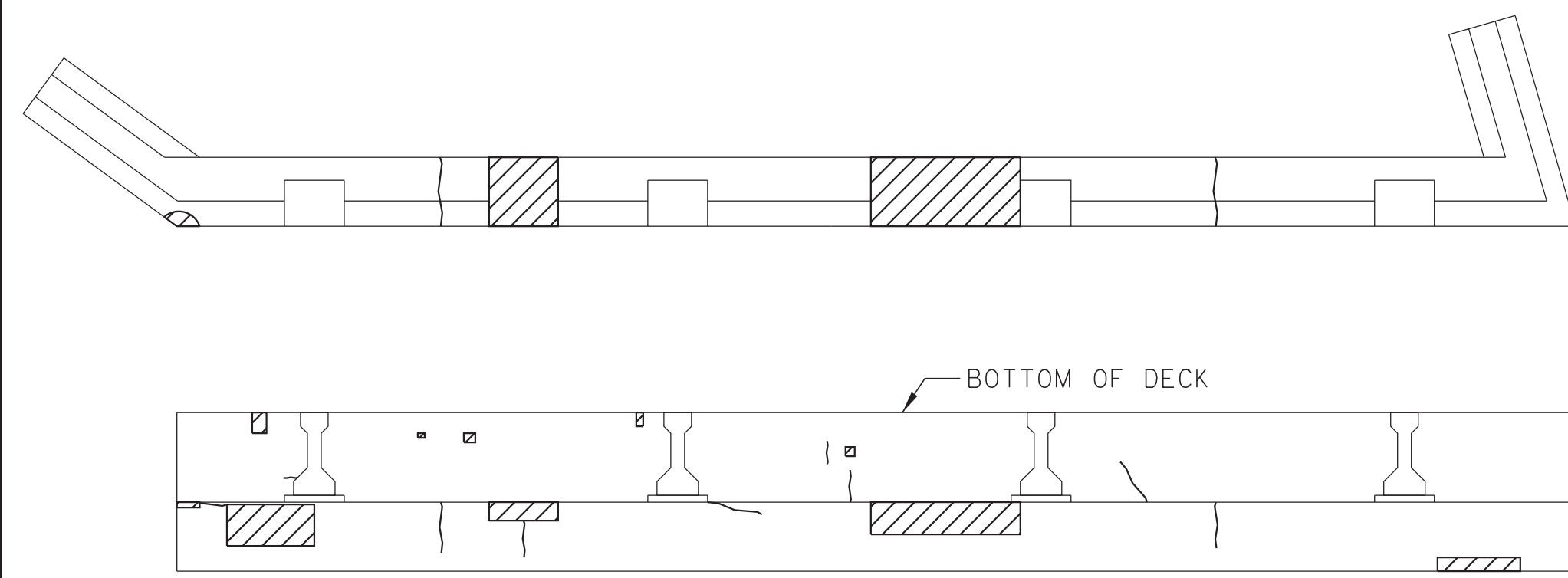
PROJECT NO.	YEAR	SHEET NO.	
181040-M3-010	2026	B5	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

ESTIMATED QUANTITIES

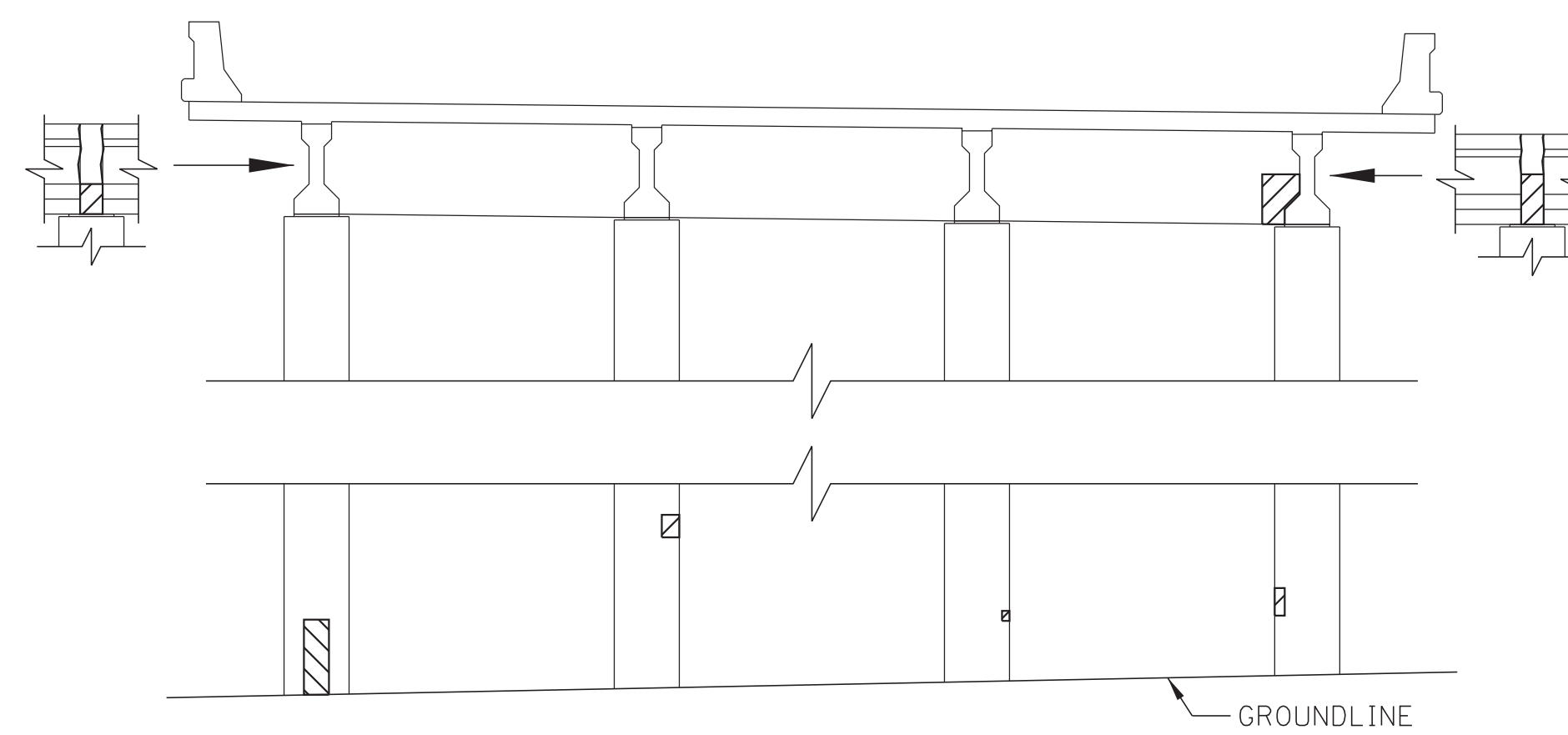
LOCATION OF REPAIR	ITEM 604-10.05 AND/OR 604-10.54 APPROX. REPAIR AREAS (S.F.)	ITEM 604-10.62 APPROX. REPAIR AREAS (L.F.)
ABUT. 1	36	18
BENT 1	26	3
BENT 2	10	6
BENT 3	6	-
ABUT. 2	2	15
TOTAL	80	42

 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. FOR DETAILS SHOWING AREAS OF EXISTING SPALLED OR DELAMINATED CONCRETE SURFACES TO BE REMOVED AND REPAIRED, SEE SHEET BR-133-336.

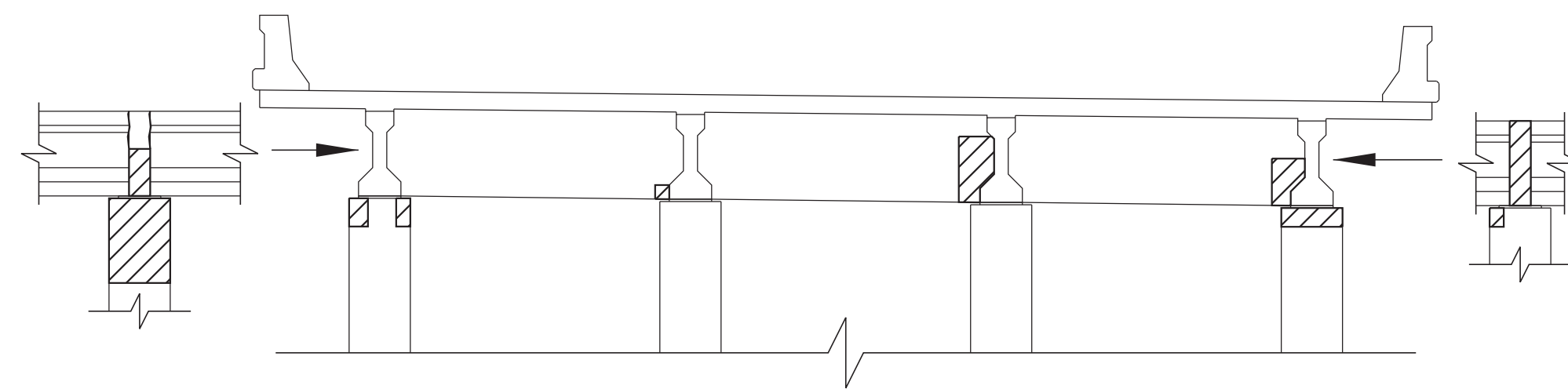
 DENOTES CRACK TO BE REPAIRED UNDER ITEM NO. 604-10.58 AND 604-10.62. SEE EPOXY INJECTION NOTES ON SHEET BR-133-336.



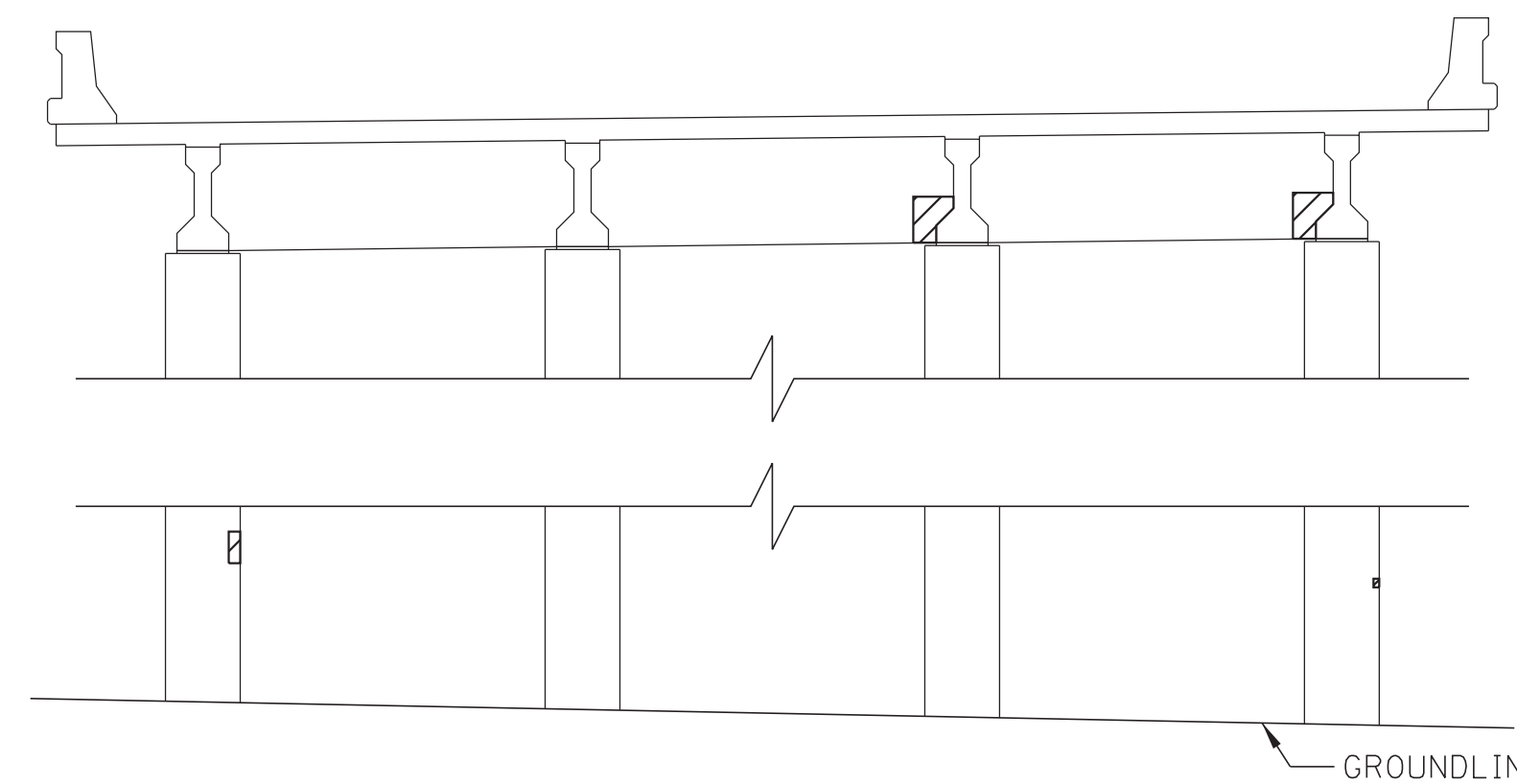
ABUTMENT 1  
LOOKING BACK



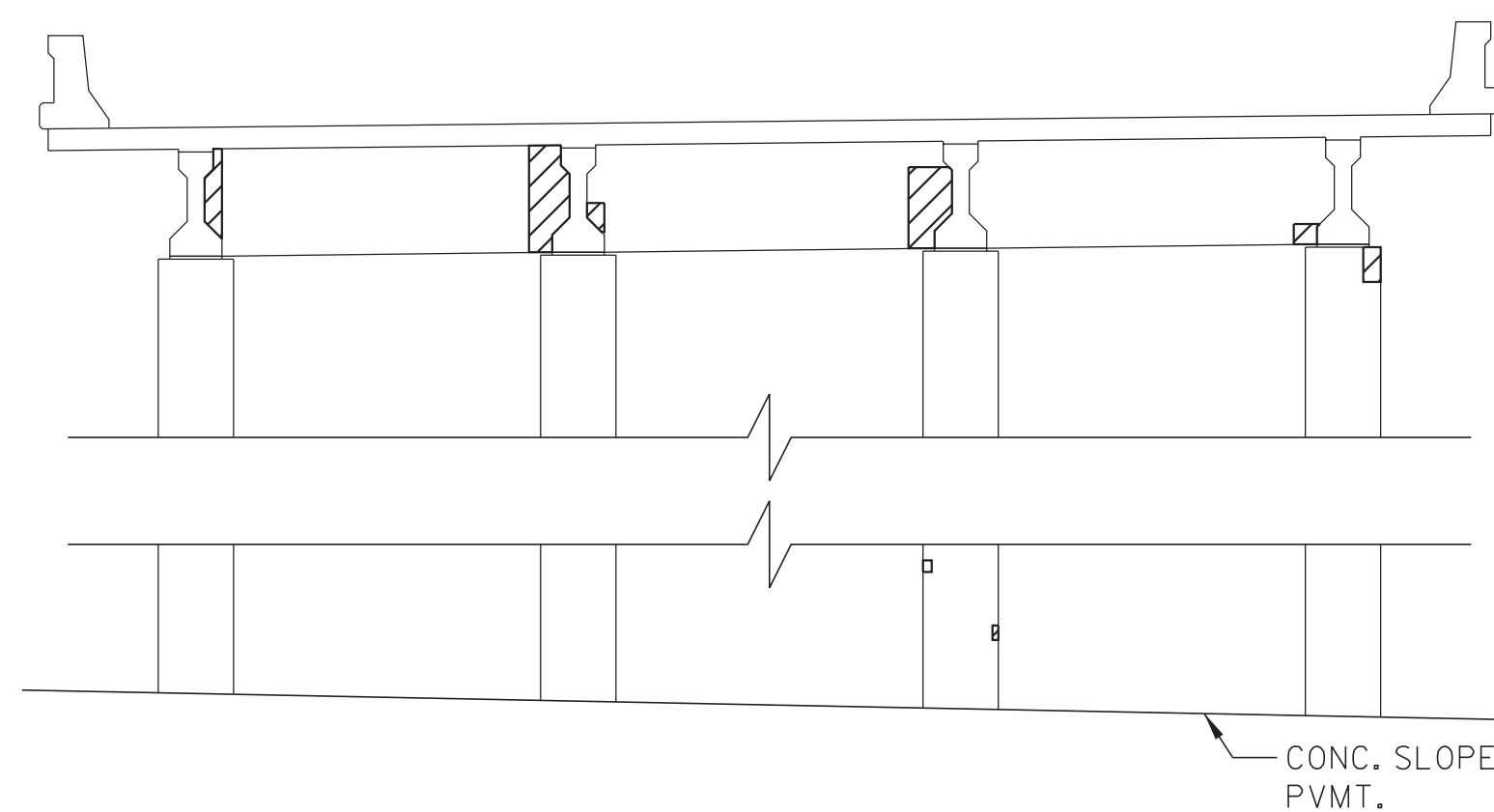
BENT 2  
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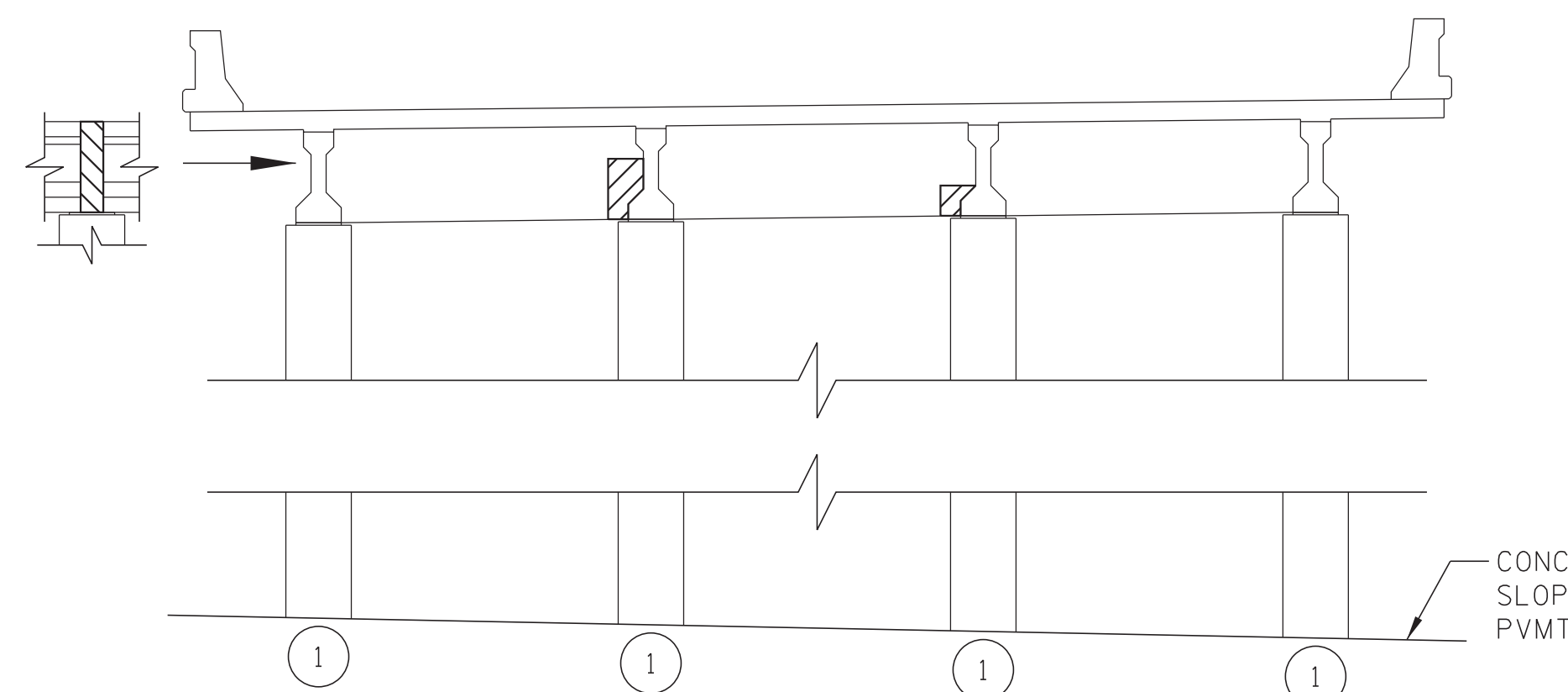
BENT 1  
LOOKING AHEAD



BENT 2  
LOOKING BACK

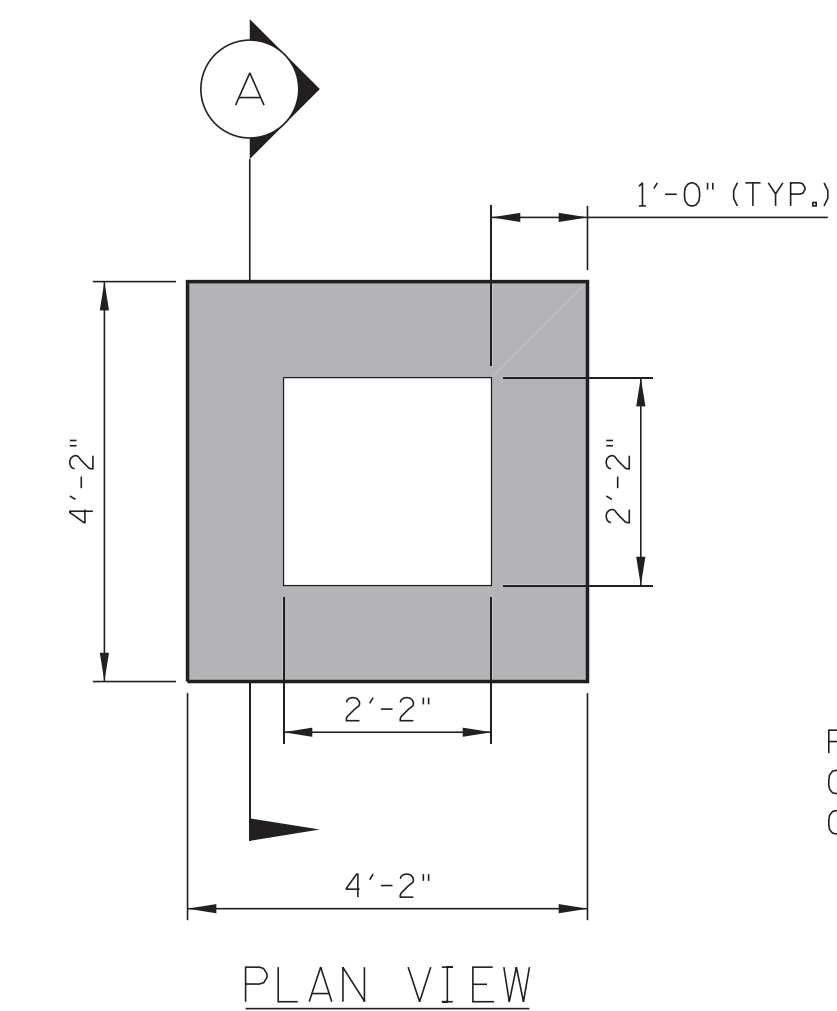


BENT 1  
LOOKING BACK

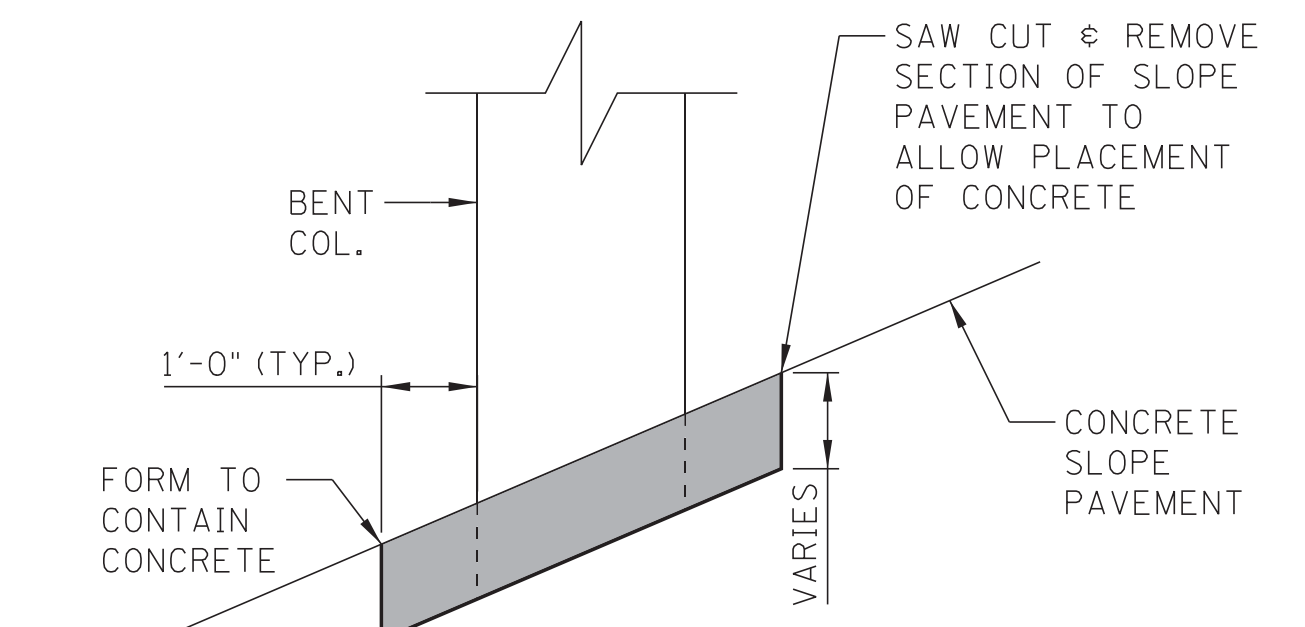


BENT 3  
LOOKING BACK

① REPAIR VOID UNDER CONCRETE SLOPE PAVEMENT. SEE SLOPE PAVEMENT VOID REPAIR THIS SHEET.



PLAN VIEW



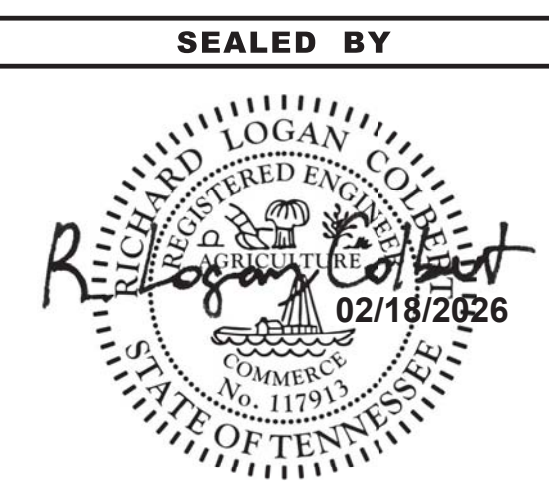
SECTION A-A

SLOPE PAVEMENT VOID REPAIR

VOID REPAIR TO BE MADE AT ALL 4 COLUMNS OF BENT 3 (EBL ONLY).

COST OF ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE REPAIR OF THE VOIDED AREAS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS), C.Y.

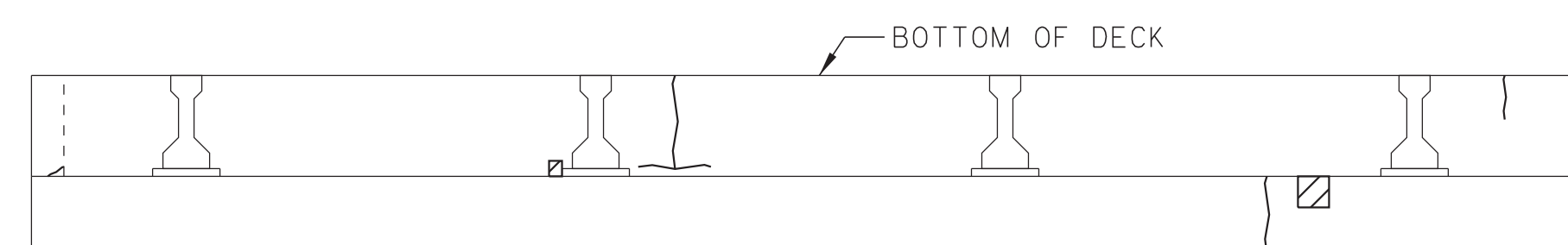
SEALED BY



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIRS (EBL)  
I-40 OVER MAIN STREET  
BRIDGE NO. 18-10040-23.92 L&R  
FED. I.D. NO. 18100400033 & 18100400034  
CUMBERLAND COUNTY  
2026

BR-133-334



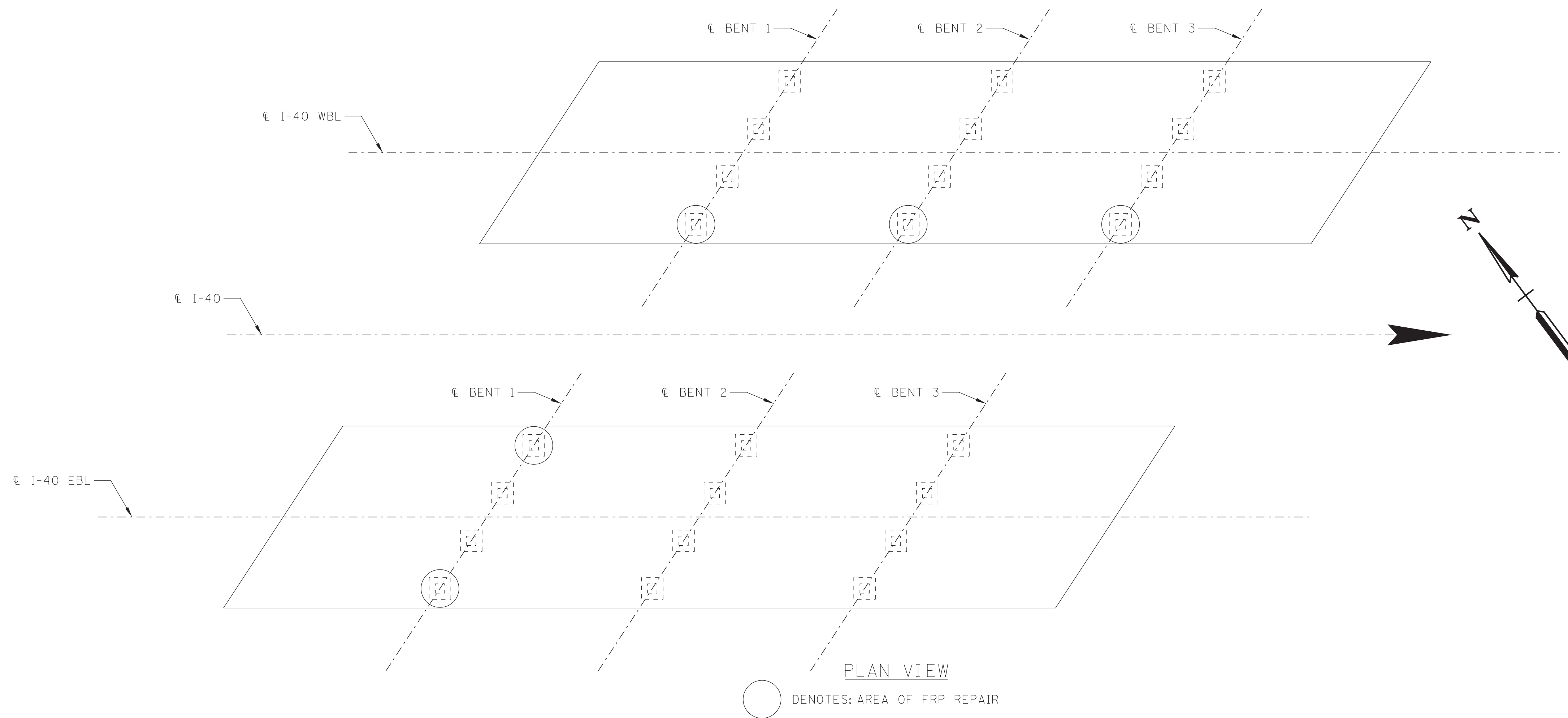
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LOOKING AHEAD

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**Palmer ENGINEERING**  
PIN NO.: 136145.00  
DESIGN BY: R.L. COLBERT DATE: 02/2026  
DRAWN BY: M.E. LOUCKS DATE: 02/2026  
SUPERVISED BY: G.S. WILSON DATE: 02/2026  
CHECKED BY: R.L. COLBERT DATE: 02/2026

CONST. NO.:

PROJECT NO.	YEAR	SHEET NO.	
181040-M3-010	2026	B6	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



**JACKING NOTES**

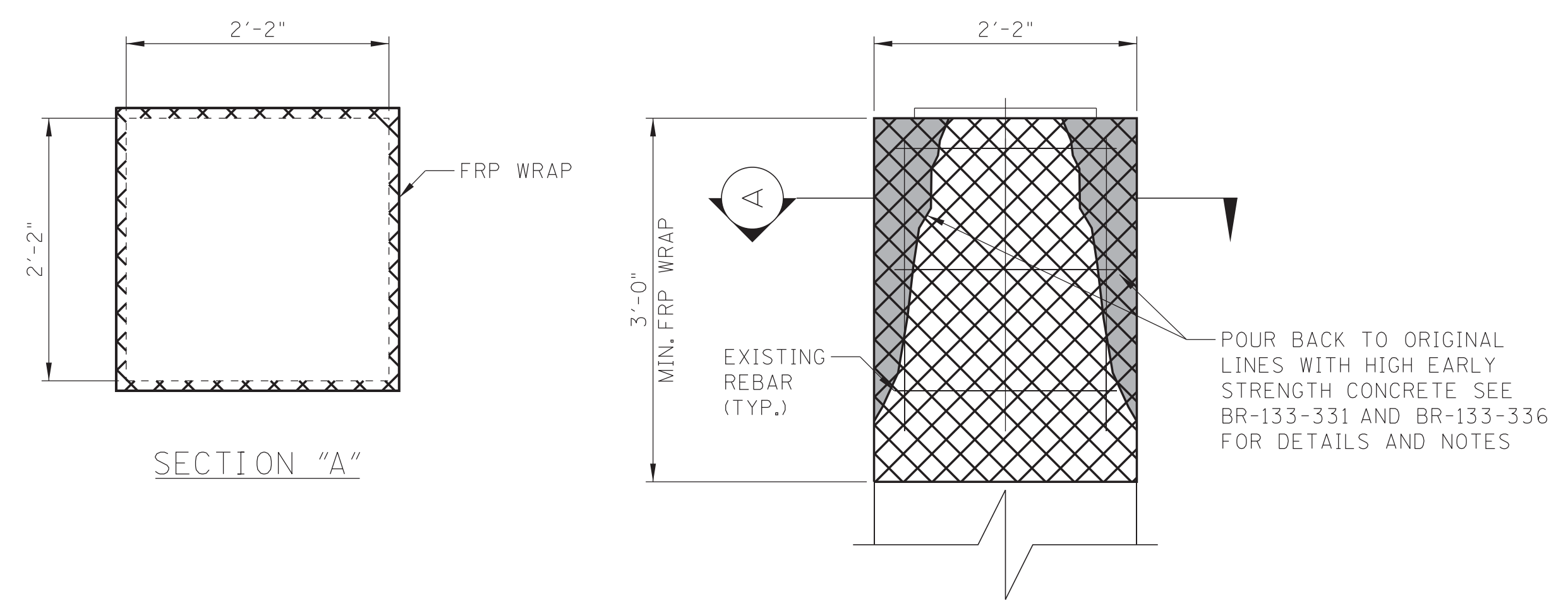
STRUCTURE IS TO BE CAREFULLY MONITORED DURING JACKING OPERATIONS SO AS NOT TO DAMAGE ANY PORTION OF THE STRUCTURE.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

JACK/SHORE BEAMS AS NECESSARY TO PERFORM REPAIRS TO BEAMS AND COLUMNS. RAISE THE STRUCTURE ONLY THE MINIMUM AMOUNT NEEDED TO PERFORM THE WORK INDICATED. COST OF JACKING STRUCTURE AND TEMPORARY SUPPORTS SHALL BE INCLUDED IN UNIT PRICE BID FOR ITEM NO. 604-10.24, JACKING CONCRETE SPANS, L.S.

JACKING/SHORING DETAILS ARE SUBJECT TO THE APPROVAL OF THE DEPARTMENT. ANY SHORING DETAILS, PROCEDURES, AND APPURTENANCES FOR JACKING OF THE STRUCTURE SHALL BE DESIGNED BY AN ENGINEER LICENSED IN TENNESSEE. THE JACKING/SHORING PLANS SHALL BE SEALED BY THE ENGINEER AND SUBMITTED TO THE HEADQUARTERS OF BRIDGE INSPECTION AND REPAIR OFFICE FOR APPROVAL. WORK WILL NOT BE ALLOWED UNTIL THE PLAN HAS BEEN APPROVED.

THE CONTRACTOR IS TO FIELD VERIFY DIMENSIONS SHOWN PRIOR TO FABRICATION.



**FRP COLUMN REPAIR NOTES:**

UPON COMPLETION OF CONCRETE REPAIRS, THE UNIDIRECTIONAL FRP SYSTEM SHALL BE INSTALLED. THE FRP SYSTEM SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS AND TDOT SPECIAL PROVISION 604FRP, AND SHALL MEET THE FULL SATISFACTION OF THE ENGINEER.

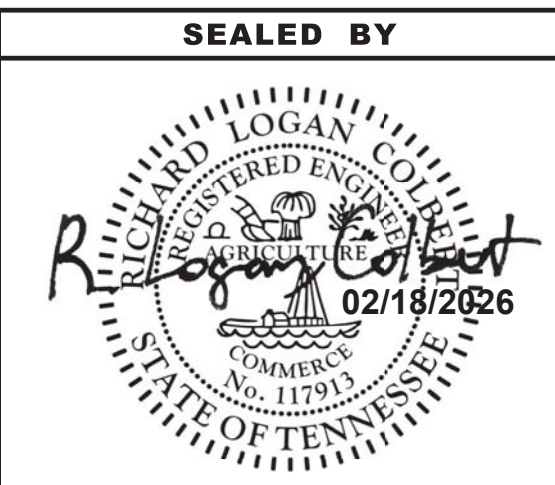
SEE TDOT SPECIAL PROVISION 604FRP FOR ALL FIBER-REINFORCED POLYMER MATERIAL AND CONSTRUCTION REQUIREMENTS INCLUDING ON-SITE REPRESENTATION FROM THE MANUFACTURER.

ALL FRP WRAP LOCATIONS ARE CONTACT-CRITICAL.

ALL CONCRETE REPAIRS RECEIVING A FIBER WRAP SHALL BE CURED A MINIMUM OF 2 DAYS BEFORE THE FIBER WRAP IS APPLIED AND REACH A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI. THE SURFACES SHALL BE FREE FROM FINES, DEPRESSIONS, OR OTHER CONDITIONS THAT MAY AFFECT THE INTENDED PERFORMANCE OF THE FRP SYSTEM. THE CERTIFIED AND EXPERIENCED INSTALLERS RESPONSIBLE SHALL VERIFY THAT ALL SURFACE PREPARATION HAS BEEN COMPLETED PROPERLY AND THAT THE FRP SYSTEM IS CLEARED FOR INSTALLATION.

THE FRP SYSTEM IS TO BE FINISHED WITH ACRYLIC PAINT MATCHING THE COLOR OF THE EXISTING TEXTURE COATING. PAINT SYSTEM TO BE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. ALTERNATE FINISHES MUST BE APPROVED BY THE TDOT BRIDGE INSPECTION AND REPAIR OFFICE.

COST OF ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO PLACE THE FRP WRAP FOR THE LIMITS SHOWN SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 604-10.83, COMPOSITE FIBER ENCASEMENT, S.F.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

FRP REPAIR DETAILS  
I-40 OVER MAIN STREET  
BRIDGE NO. 18-I0040-23.92 L&R  
FED. I.D. NO. 18100400033 & 18100400034  
CUMBERLAND COUNTY  
2026

BR-133-335

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<b>Palmer</b> ENGINEERING	PIN NO: 136145.00	DATE: 02/2026
DESIGN BY: R.L. COLBERT	DATE: 02/2026	
DRAWN BY: M.E. LOUCKS	DATE: 02/2026	
SUPERVISED BY: G.S. WILSON	DATE: 02/2026	
CHECKED BY: R.L. COLBERT	DATE: 02/2026	

CONST. NO.:

PROJECT NO.	YEAR	SHEET NO.	
181040-M3-010	2026	B7	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

SPECIAL NOTES FOR EPOXY INJECTION

UNLESS OTHERWISE NOTED, THE INTENT OF THIS SPECIFICATION IS FOR DESIGNATED CRACKS TO BE INJECTED THEIR FULL LENGTH AND DEPTH.

DESIGNATED CRACKS SHALL BE INJECTED WITH AN APPROVED EPOXY RESIN ADHESIVE FILLING ALL VOIDS FOR THE CRACK DEPTH OR THICKNESS OF THE MEMBER. THE EPOXY RESIN ADHESIVE SHALL BE ON THE CURRENT QUALIFIED PRODUCTS LIST MAINTAINED BY THE DIVISION OF MATERIALS AND TEST. ALL CRACKS SHALL BE INJECTED USING AN ADHESIVE SUITABLE FOR THE FIELD CONDITIONS (CRACK WIDTH, TEMPERATURE, HUMIDITY, ETC.) RECOMMENDED BY THE ADHESIVE MANUFACTURER AS SHOWN ON MATERIAL DATA SHEETS. FOLLOWING INJECTION, ALL INJECTION PORTS AND CAPPING MATERIAL SHALL BE REMOVED FROM EXPOSED SURFACES LEAVING THE SURFACE SMOOTH AND FLUSH WITH THE SURROUNDING CONCRETE SURFACES. THIS WORK ALSO INCLUDES MAKING ANY EXISTING INJECTION PORTS (FROM A PREVIOUS REPAIR PROJECT) SMOOTH AND FLUSH WITH THE SURROUNDING CONCRETE SURFACES.

THE CONTRACTOR SHALL HAVE SUFFICIENT EXPERIENCE AND TRAINING TO PERFORM THE EPOXY INJECTION IN ACCORDANCE WITH THESE PLANS. PRIOR TO PERFORMING ANY WORK, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A WRITTEN PROCEDURE FOR PERFORMING THE EPOXY INJECTION. THE PROCEDURE SHALL DESCRIBE IN DETAIL HOW THE WORK WILL BE PERFORMED. THE PROCEDURE SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING INFORMATION:

- 1) DESCRIPTION OF EQUIPMENT.
  - A. THE INJECTION EQUIPMENT SHALL BE OF THE TYPE THAT MIXES ADHESIVE COMPONENTS AT THE INJECTION HEAD.
  - B. THE INJECTION EQUIPMENT SHALL BE CAPABLE OF DISCHARGING MIXED ADHESIVE AT ANY PRESSURE UP TO 300 PSI. THE INJECTION EQUIPMENT SHALL BE EQUIPPED WITH GAUGES WHICH CAN MEASURE THE INJECTION PRESSURE AND VOLUME.
- 2) EQUIPMENT CALIBRATION PROCEDURES AND SCHEDULE.
- 3) MATERIALS TO BE USED (INCLUDING MANUFACTURER DATA SHEETS).
  - A. CAPPING MATERIAL
  - B. EPOXY ADHESIVE (TYPE TO BE APPROPRIATE FOR CRACK SIZES TO BE INJECTED).
- 4) PORT SPACING
  - A. PORT SPACING SHALL NOT BE LESS THAN THE THICKNESS OF THE CONCRETE IN THAT LOCATION.
- 5) INJECTION SEQUENCE
  - A. INJECTION SHALL PROCEED FROM LOWER END OF CRACK ALONG ADJACENT PARTS.
  - B. SKIPPING OF PORTS DURING INJECTION SHALL NOT BE ALLOWED.

THE CONTRACTOR SHALL HAVE THE MANUFACTURER'S INSTRUCTIONS FOR PROPORTIONING AND MIXING AVAILABLE AT THE JOB SITE AT ALL TIMES AND SHALL ENSURE THAT THE EQUIPMENT IS SUPPLYING THE MIXED ADHESIVE IN THE CORRECT PROPORTIONS.

TO ENSURE PROPER MIXING AND PROPORTIONING, SAMPLES SHALL BE TAKEN FROM THE INJECTOR HEAD. SAMPLES SHALL BE TAKEN AT THE START OF EACH WORKDAY AND EACH TIME THE ADHESIVE RESERVOIRS ARE REFILLED. THE SAMPLES SHALL BE IN A TEST CUP. THE SAMPLE SHALL BE MONITORED TO ENSURE THAT THE CURE TIME IS IN COMPLIANCE WITH THE MANUFACTURER'S DATA SHEETS. IF THE SAMPLES DO NOT CURE IN THE SPECIFIED TIME THEN THE EQUIPMENT USED TO PRODUCE THE SAMPLE SHALL NOT BE USED UNTIL THE PROBLEM IS CORRECTED.

CORE SAMPLES SHALL BE TAKEN AS VERIFICATION OF THE QUALITY OF WORK. THE CONTRACTOR SHALL TAKE ONE (1) TWO (2) INCH DIAMETER (FULL DEPTH OF CONCRETE AT LOCATION CORED) CORE SAMPLE STARTING WITH THE FIRST REPAIR LOCATION THEN EVERY TENTH REPAIR LOCATION AFTERWARDS WORK SHALL NOT PROCEED UNTIL THE CORE SAMPLE IS TAKEN AND ACCEPTED. ALL CORE SAMPLES AND HOLES SHALL BE INDEXED FOR FUTURE REFERENCE. THE ENGINEER SHALL DESIGNATE ALL LOCATIONS TO BE CORED. IF ANY CORES SHOW UNACCEPTABLE RESULTS, ALL WORK SHALL BE STOPPED UNTIL THE CONTRACTOR SUBMITS A PROPOSAL FOR CORRECTING UNACCEPTABLE WORK.

THE INITIAL CORE WILL ALSO SERVE TO QUALIFY THE FOREMAN FOR THIS WORK. IF AT ANY TIME A NEW FOREMAN IS USED, HE SHALL BE QUALIFIED WITH A CORE SAMPLE.

THE CONTRACTOR, AT HIS EXPENSE, SHALL REPAIR ALL CORE HOLES WITH AN APPROVED CEMENTITIOUS PATCHING MATERIAL.

CORE SAMPLES SHALL BE VISUALLY INSPECTED TO CONFIRM THAT CRACKS ARE COMPLETELY FILLED WITH ADHESIVE. ANY CORE HAVING LESS THAN 95% OF THE CRACK FILLED WITH ADHESIVE SHALL BE CONSIDERED UNACCEPTABLE AND BE REJECTED.

CORE SAMPLES SHALL BE TESTED FOR BOND STRENGTH. SAMPLES MAY BE FRACTURED BY HAMMER BLOW TO CRACK AREA OR THROWN AT A HARD SURFACE. IF ADHESIVE FAILURE OCCURS BEFORE CONCRETE FAILURE, THE CORE SHALL BE CONSIDERED UNACCEPTABLE AND REJECTED.

PAYMENT FOR EPOXY INJECTION CRACK REPAIR SHALL BE MADE UNDER ITEM NUMBERS:

- 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE), L.F. AND
- 604-10.58, EPOXY INJECTION (INJECTION), GAL

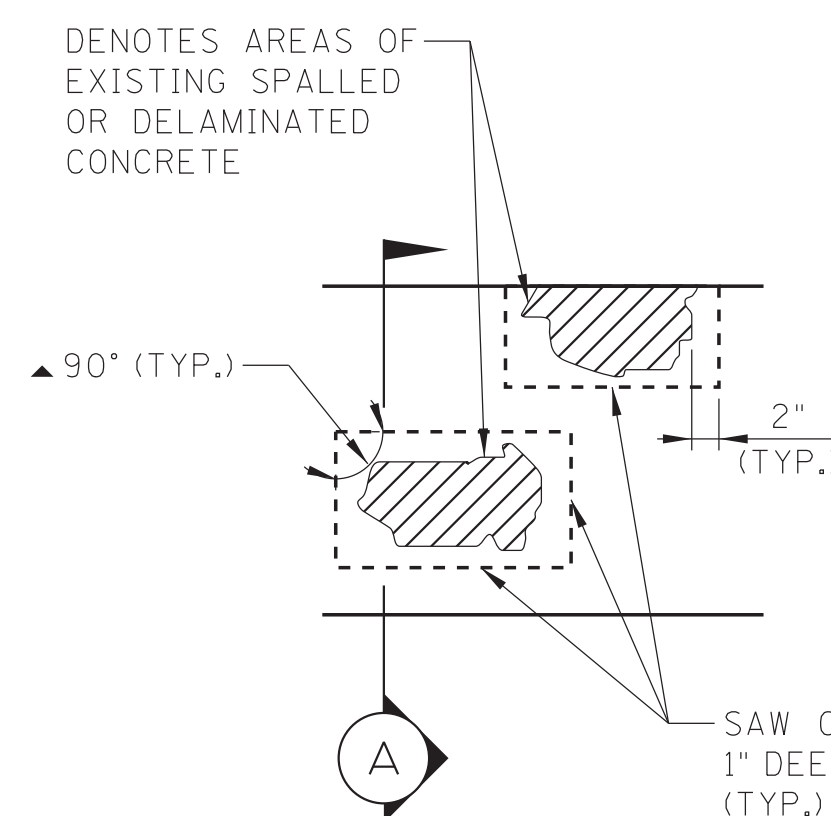
PRICE BID FOR ITEM NUMBER 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE), L.F., SHALL INCLUDE COST OF ALL LABOR AND MATERIALS (EXCEPT ADHESIVE) FOR GRINDING FOR SURFACE PREPARATION, CRACK PREPARATION, CAPPING, INJECTION OF ADHESIVE, ALL SAMPLING AND TESTING, REMOVAL OF CAPPING MATERIAL AND PORTS, AND OTHER INCIDENTALS. CRACKS SHALL BE MEASURED FOR PAYMENT ALONG THE LENGTH OF THE VISIBLE SURFACE CRACK.

PRICE BID ALSO INCLUDES ALL COSTS ASSOCIATED WITH MAKING THE EXISTING INJECTION PORTS (FROM A PREVIOUS REPAIR PROJECT) SMOOTH AND FLUSH WITH THE SURROUNDING CONCRETE SURFACE.

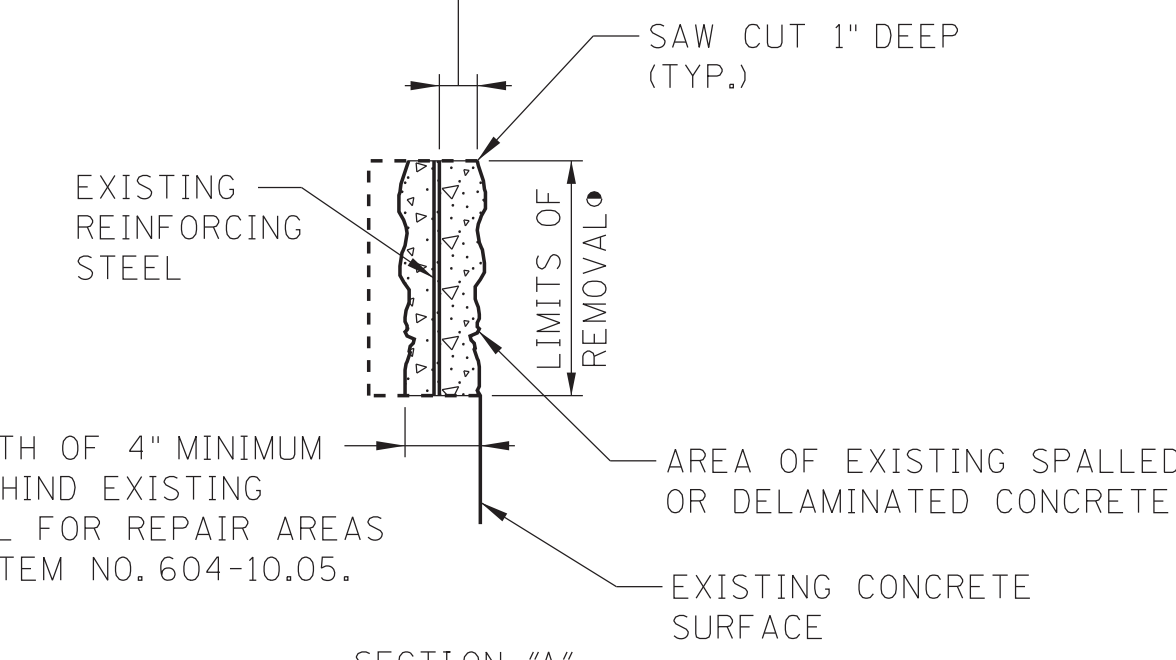
PRICE BID FOR ITEM NUMBER 604-10.58, EPOXY INJECTION (INJECTION), GAL, SHALL INCLUDE COST FOR ADHESIVE MATERIAL INJECTED ONLY.

NO PAYMENT SHALL BE MADE FOR REWORK DEEMED NECESSARY BY FAILURE OF ADHESIVE SAMPLES OR CORE SAMPLES.

ALL WORK INCLUDING SAMPLING AND TESTING SHALL BE IN THE PRESENCE OF THE ENGINEER OR HIS REPRESENTATIVE OR CONTRACT INSPECTORS. ANY WORK DONE WITHOUT INSPECTORS PRESENT SHALL NOT BE PAID FOR. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH WEEKLY SCHEDULES OF WORK TO BE PERFORMED. SCHEDULES SHALL BE SUBMITTED AT LEAST THREE (3) DAYS IN ADVANCE OF WORK TO BE DONE. THE ENGINEER SHALL BE NOTIFIED OF ANY CHANGE IN THE SCHEDULE A MINIMUM OF TWENTY-FOUR (24) HOURS IN ADVANCE OF CHANGE.



REMOVE ALL DETERIORATED LOOSE CONCRETE TO A MINIMUM DEPTH OF 2" FOR REPAIR AREAS PAID FOR UNDER ITEM NO. 604-10.54



DETAILS SHOWING AREAS OF EXIST. SPALLED OR DELAMINATED CONCRETE SURFACES TO BE REMOVED AND REPAIRED

- DENOTES: LIMITS AND LOCATION OF REPAIRS TO BE DESIGNATED BY THE ENGINEER.
- ▲ DENOTES: SAW CUT EXISTING CONCRETE SURFACES SO AS TO OBTAIN SQUARED CORNERS.

EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED OR DELAMINATED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED EXISTING REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.

THE ENGINEER SHALL HAVE THE OPTION OF DESIGNATING A SPALLED OR DELAMINATED AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 OR 604-10.54. PATCHING MATERIAL FOR ITEM NO. 604-10.05 SHALL BE HIGH EARLY STRENGTH CONCRETE. PATCHING MATERIAL FOR ITEM NO. 604-10.54 SHALL BE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL. SEE QUALIFIED PRODUCTS LIST (SECTION B, OPL 13.009).

COST OF SAW CUTTING, REMOVING SPALLED OR DELAMINATED CONCRETE, CLEANING, PATCHING MATERIAL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN TO BE INCLUDED IN ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F. OR ITEM NO. 604-10.05, CONCRETE, S.F.

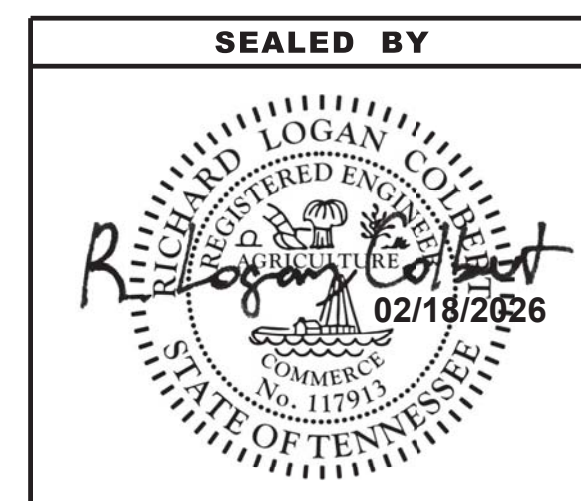
DURING SUBSTRUCTURE REPAIRS, ANY EXISTING REINFORCING STEEL EXHIBITING MORE THAN 20 PERCENT SECTION LOSS SHALL BE REPAIRED. EITHER NEW IN-KIND REINFORCING SHALL BE SPLICED TO THE EXISTING BARS OR THE BARS SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. THE COST OF ANY NEW REINFORCING STEEL REQUIRED TO COMPLETE THESE REPAIRS SHALL BE INCLUDED IN ITEM NO. 604-10.05, CONCRETE, S.F. ANY DAMAGE TO EXISTING REINFORCING STEEL DURING CONCRETE REMOVAL SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

THE ENGINEER SHALL DESIGNATE ALL SPALLED OR DELAMINATED CONCRETE REPAIR AREAS IN THE FIELD. QUANTITIES GIVEN ARE APPROXIMATE.

POWER DRIVEN HAND TOOLS USED FOR REMOVAL OF UNSOUND CONCRETE ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

1. PNEUMATIC HAMMERS HEAVIER THAN THE 35 LB. CLASS SHALL NOT BE USED.
2. CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BEHIND REINFORCING STEEL.

PNEUMATICALLY PLACED CONCRETE IS NOT ALLOWED.



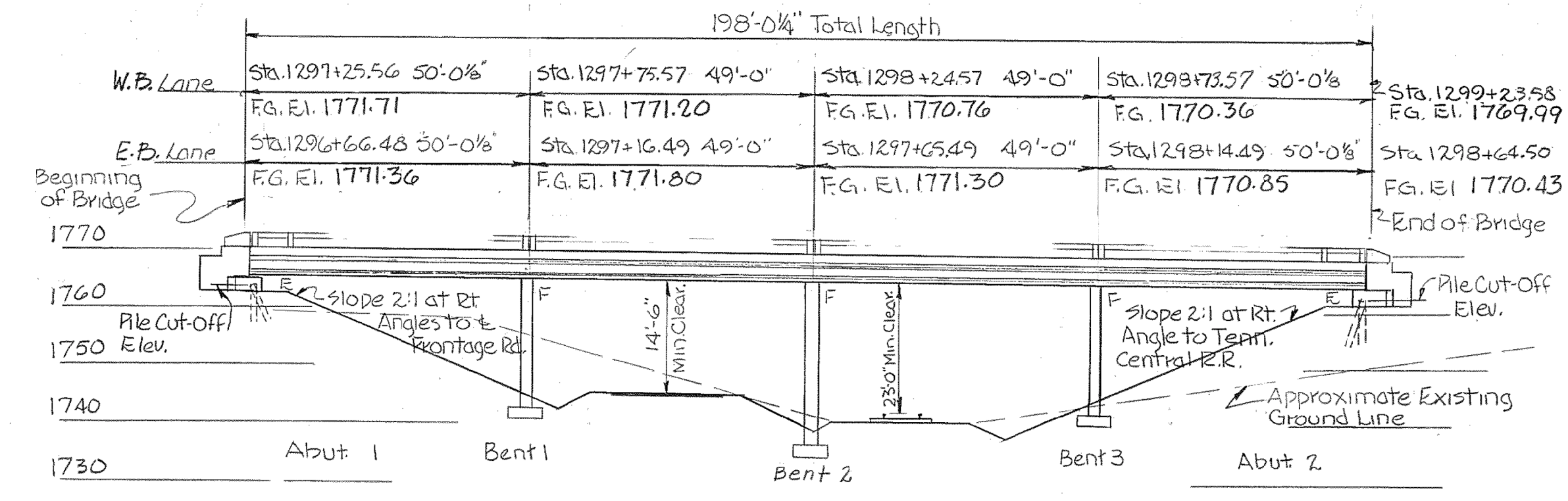
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIR DETAILS  
I-40 OVER MAIN STREET  
BRIDGE NO. 18-I0040-23.92 L&R  
FED. I.D. NO. 18100400033 & 18100400034  
CUMBERLAND COUNTY  
2026

BR-133-336

<b>Palmer</b> ENGINEERING	PIN NO.: 136145.00	
DESIGN BY: R.L. COLBERT	DATE: 02/2026	
DRAWN BY: M.E. LOUCKS	DATE: 02/2026	
SUPERVISED BY: G.S. WILSON	DATE: 02/2026	
CHECKED BY: R.L. COLBERT	DATE: 02/2026	

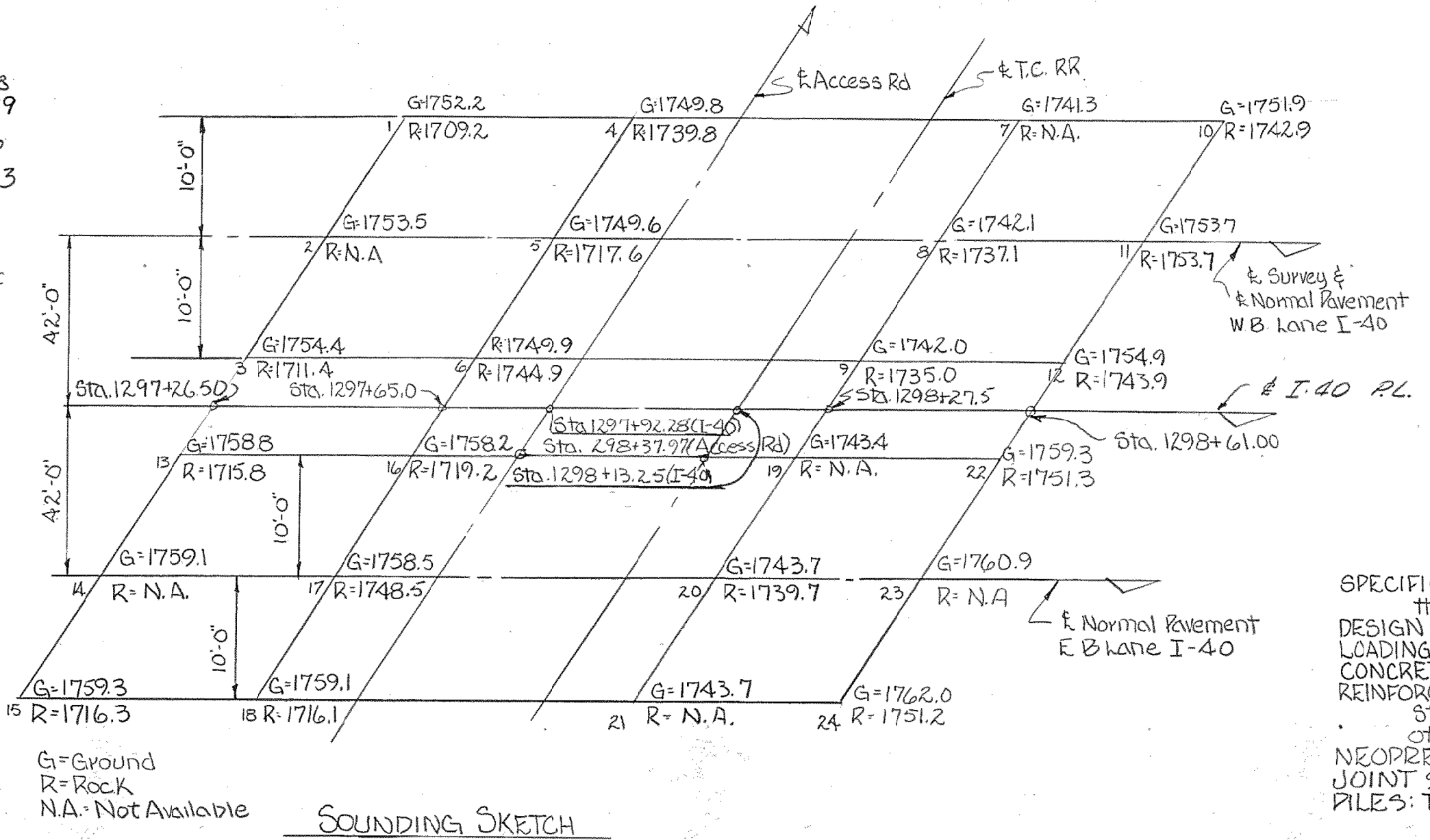
Rev. 2-Feb-66 Bridge moved 3" & clearance note rev.  
 Rev 28-Feb-66 Sta moved from bridge to E Lane



Note: Fills at ends of bridge shall be in place and thoroughly compacted before Abutment piles are driven.

ELEVATION AT RT. ANGLE TO I-40

Note: E=Expansion  
 F=Fixed



GENERAL NOTES

SPECIFICATIONS: Standard Road and Bridge Specifications of the Tennessee Dept. of Highways.  
 DESIGN SPECIFICATIONS: 1961 AASHTO Specifications, with addendums.  
 LOADING: HS20-44 & IT Mill Tary.  
 CONCRETE: To be Class 'A' (Cast in place)  
 REINFORCING STEEL: To be intermediate or hard grade. See Specifications. Standard Hook Details as recommended by C.R.S.I. shall apply, unless otherwise noted.  
 NEOPRENE BEARING PADS: See Special Provisions.  
 JOINT SEALER: See Special Provisions, Class 'A' or 'B'.  
 PILES: To be 10" BP42 Steel Piles.

FOUNDATION NOTE

Foundations for Bents shall be excavated to the Pile Cut-off Elevation shown. Red soundings shall then be made as directed by the Engineer. From the results obtained the Engineer will decide if Piles are to be used or the footing carried to rock. Piles shall be driven to rock or a minimum bearing of 40 tons for Bents and Abutments. No reinforcing steel for columns shall be ordered until final footing elevations are established. Cost of red soundings shall be included in the cost of items bid on.

BRIDGERAIL NOTE

Build Bridgerail according to Standard Dwg. No. K-38-151 for Concrete Bridgerail. See also Dwg. No. K-51-120 Endpost Details.  
 G=195'-5" (Typical E.B. & W.B. Lanes)

CONC. BRIDGERAIL	Dimensions	
	L	No. Spaces
SPAN 1	6'-10 3/4"	6
SPAN 2	6'-11 7/8"	6
SPAN 3	6'-11 7/8"	6
SPAN 4	6'-10 3/4"	6

LIST OF DRAWINGS

Drawings:	No.
Layout	K-51-117
Superstructure	K-51-118
Prestressed Beam Details	K-51-119
Abutments 1&2	K-51-120
Bents 1-2-3	K-51-121
Bill of Steel	K-51-122
Bridgerail Details (See Note This Sheet)	K-38-151
Pile Splice Details	H-5-111

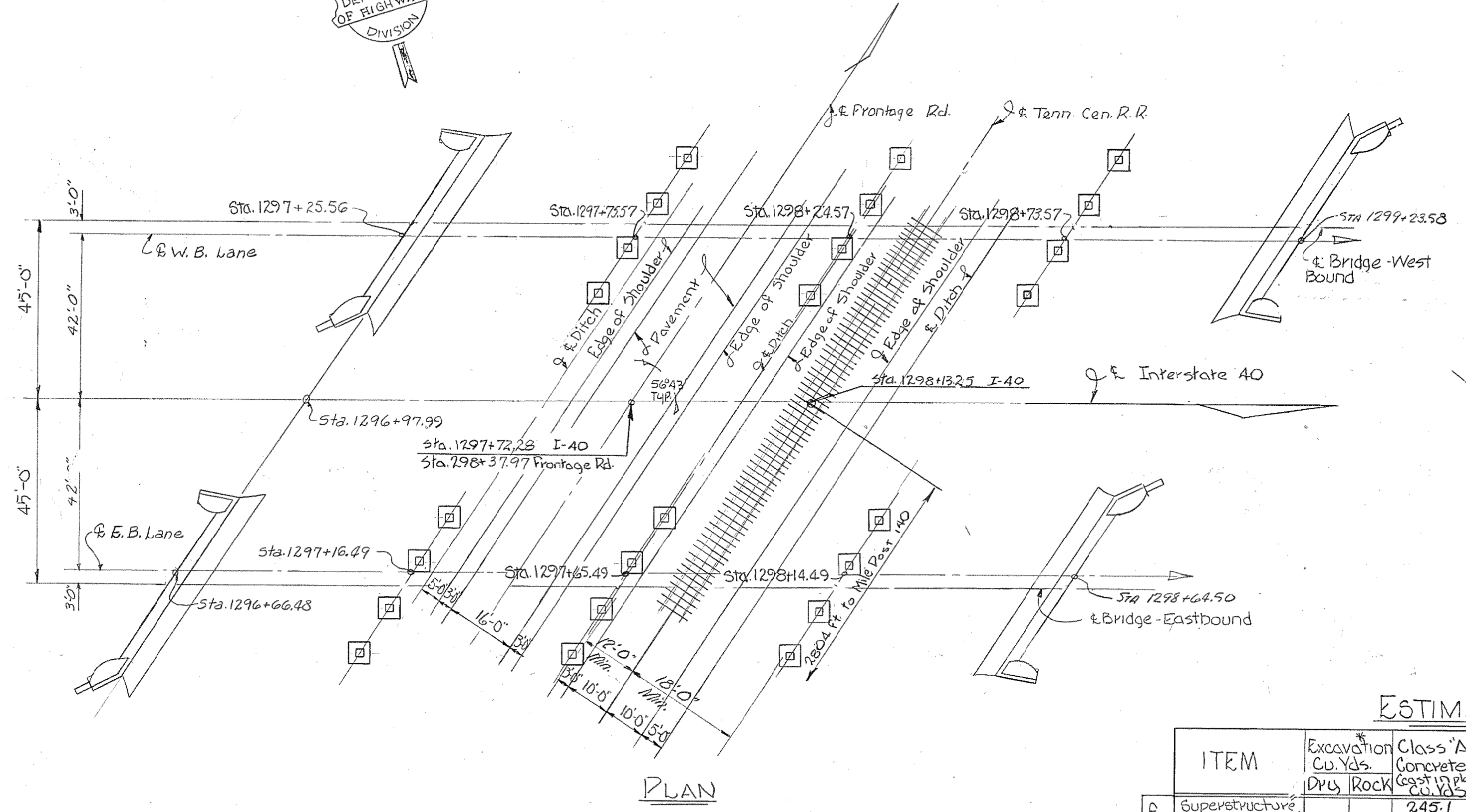
CONCRETE FINISH

In addition to any other requirement of the specifications, the face of beams and outside face of Curbs adjacent to median shall be rubbed.

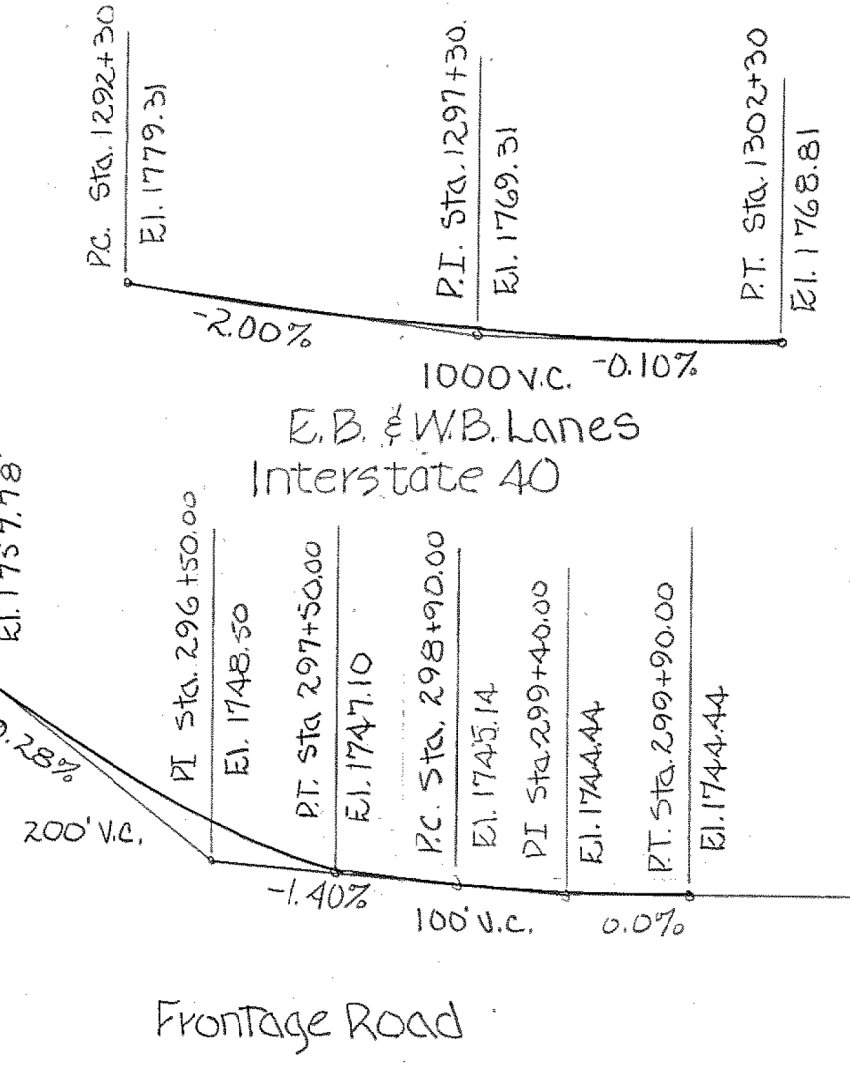
38'-0" ROADWAY WITH BUFFER CURBS

STATE OF TENNESSEE  
 DEPARTMENT OF HIGHWAYS  
 NASHVILLE

LAYOUT OF BRIDGE  
 INTERSTATE 40 OVER FRONTAGE ROAD 3  
 TENNESSEE CENTRAL RAILROAD  
 STATION 1296+98.24  
 CUMBERLAND COUNTY  
 1965



SPECIAL NOTE  
 The contractor shall conduct his work so as to protect the Railway tracks and properties from any damage. The work shall be done in accordance with the regulations stipulated by the Tennessee Central Railway so as to maintain clearances and not interrupt traffic.



ESTIMATED QUANTITIES

ITEM	Excavation Cu. Yds.	Class 'A' Concrete Cu. Yds.	Reinforc. Steel Lbs.	Concrete Bridgerail Lin. Ft.	Piles 10 BP42 Lin. Ft.	Unseeded Oil Treatment Sq. Yds.	Rock Drilling Lin. Ft.	Type II Prestressed Beams L=48'-3"
Superstructure		245.1	73,075					16
Abutment No. 1		18.7	2,337					
Bent No. 1	32	34	29.0	4,950				
Bent No. 2	18	6	31.6	5,446				
Bent No. 3	74	6	31.3	5,391				
Abutment No. 2		18.7	2,337					
Total	124	46	374.4	93,636	391	461	318	16
Superstructure		245.1	73,075					16
Abutment No. 1		18.7	2,337					
Bent No. 1	61	34	29.7	5,081				
Bent No. 2	18	6	32.7	5,631				
Bent No. 3	48	6	32.0	5,500				
Abutment No. 2		18.7	2,337					
Total	127	46	376.9	94,261	391	360	318	16
Both Lanes	251	92	751.3	187,697	782	821	636	32

#Note: Excavation based on lower profile grade.

DESIGNED BY Don Shanklin DATE 5-65  
 DRAWN BY R.E. Rowlett DATE 6-22-65  
 TRACED BY DATE  
 CHECKED BY Don Shanklin DATE 10-65

CORRECTED  
 BRIDGE ENGINEER  
 PROVED  
 STATE HIGHWAY ENGINEER

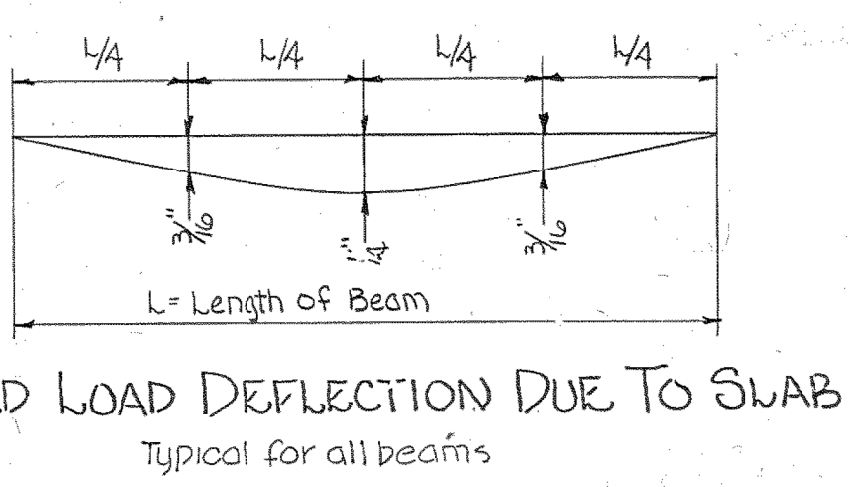
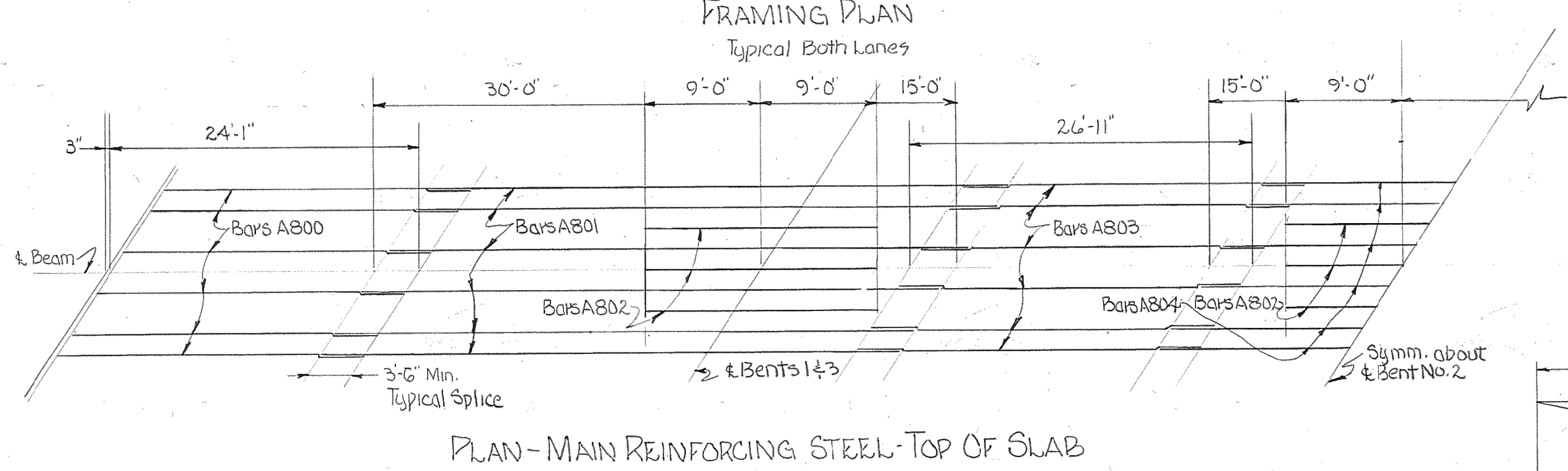
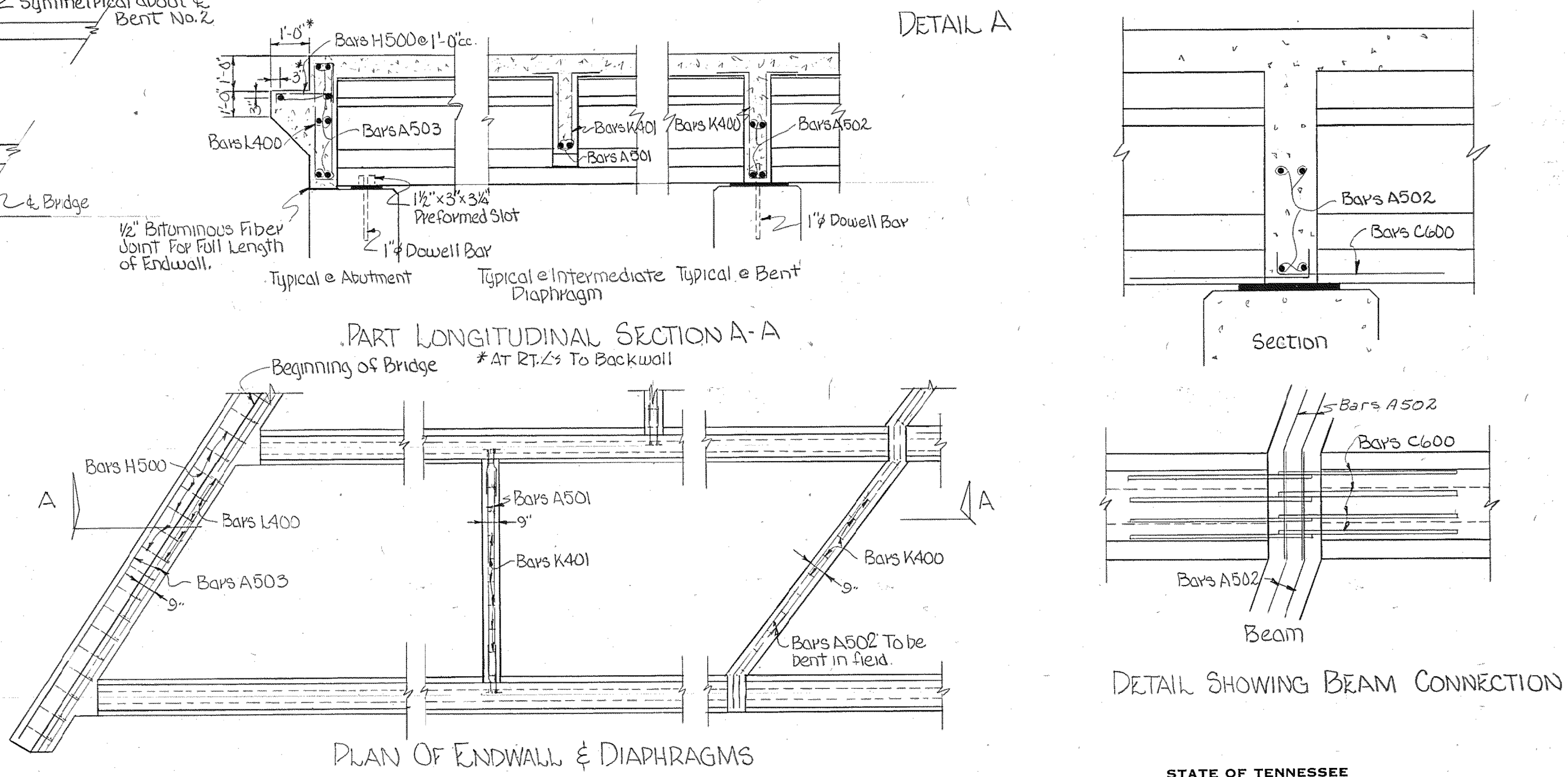
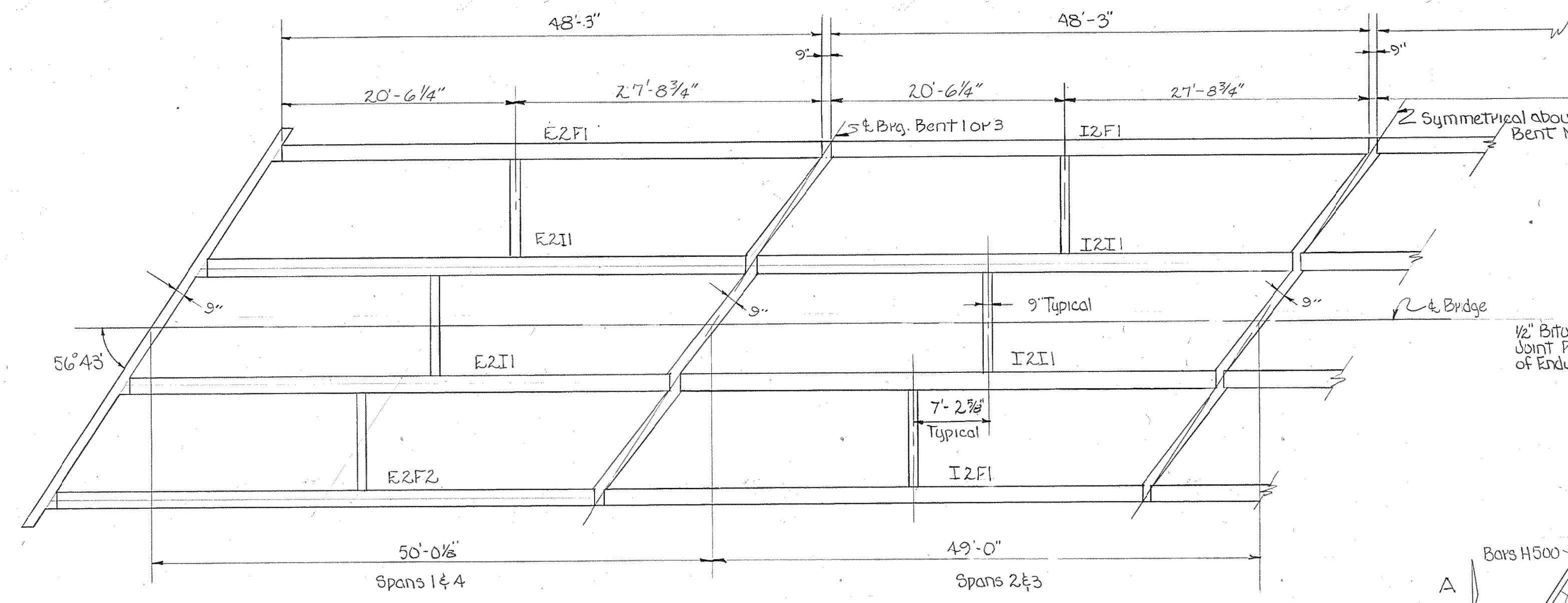
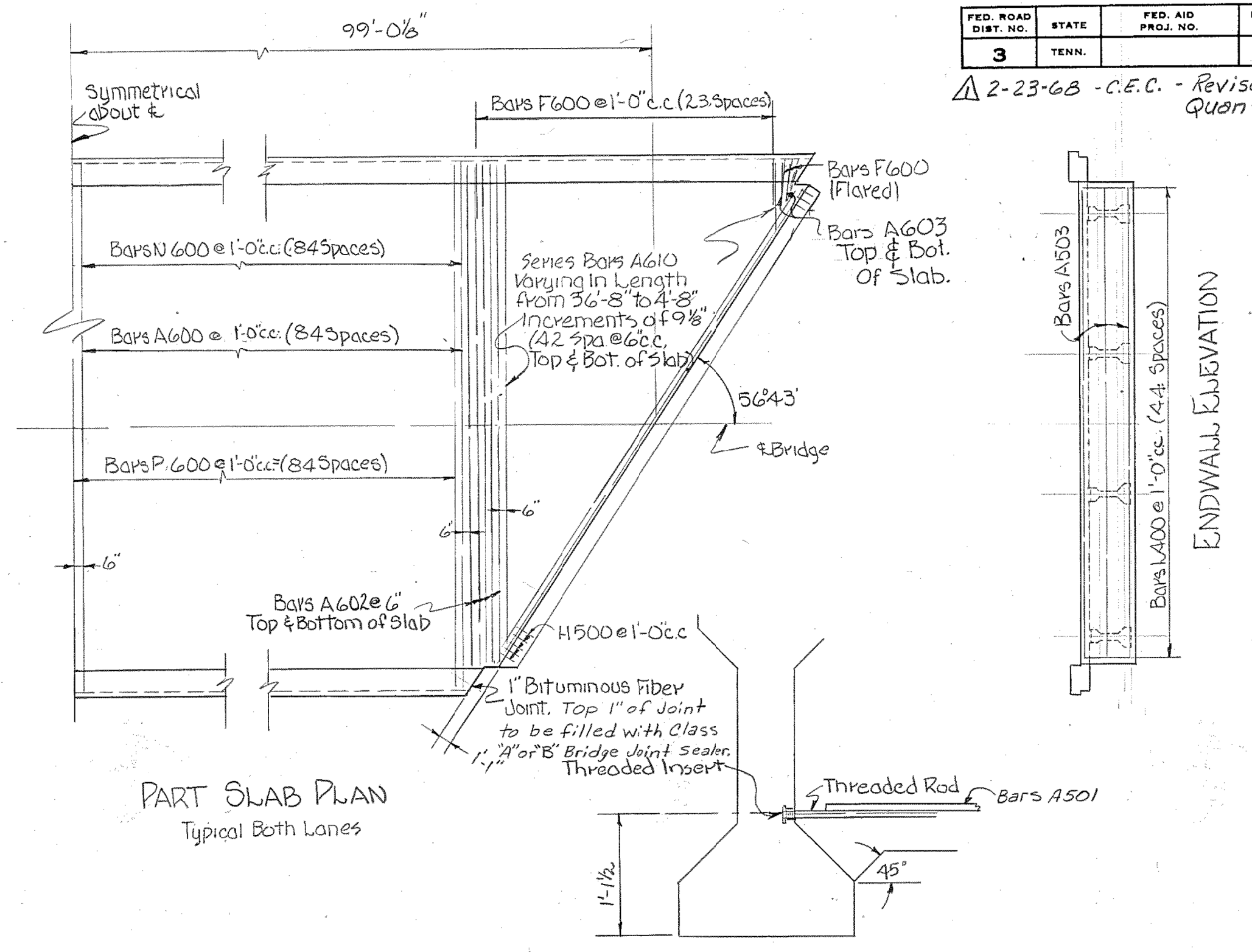
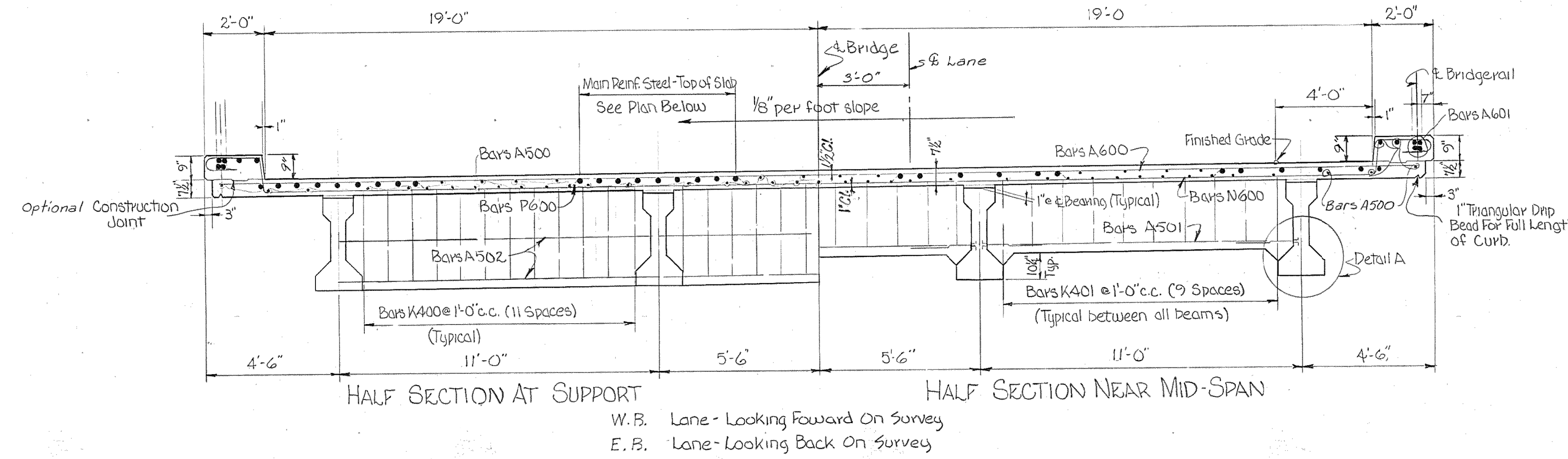
K-51-117

MICROFILMED

2-23-68 - C.E.C. - Revised Concrete Quantities.

Slab Note: Slab construction joints may be located at the contractor's option. However no equipment shall be allowed on the bridge until all pours are made and the concrete is properly cured.

Note: When pouring curbs provisions shall be made for setting anchor bars for bridge-rail. For location of anchor bars see standard bridge-rail drawing-K-98-181 & K-51-117.



ESTIMATED QUANTITIES

ITEM	Concrete Class	Reinf. Steel
W.B. Lane	245.1 cu yd	73,075 lbs.
E.B. Lane	245.1 cu yd	73,075 lbs.

STATE OF TENNESSEE  
DEPARTMENT OF HIGHWAYS  
NASHVILLE

SUPERSTRUCTURE DETAILS  
INTERSTATE 40 OVER FRONTAGE ROAD  
& TENNESSEE CENTRAL RAILROAD  
STATION 1296+98.24  
CUMBERLAND COUNTY  
-1965-

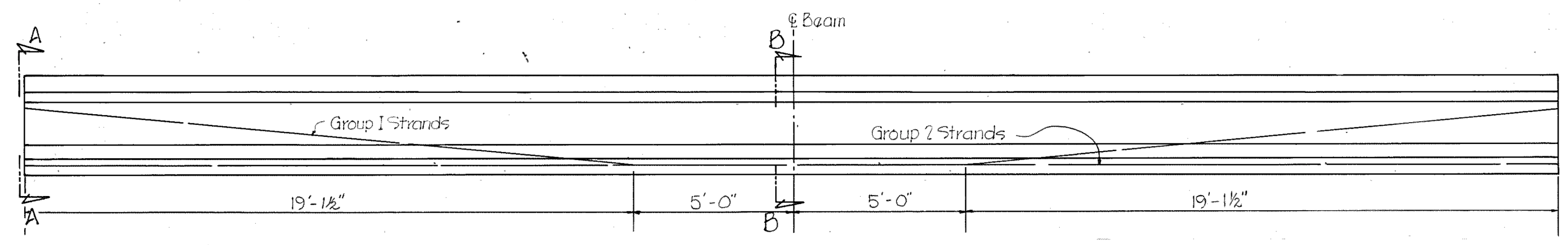
DESIGNED BY: Don Shanklin  
DRAWN BY: Robert Rowlett  
TRACED BY:  
CHECKED BY: Don Shanklin

DATE: 5-65  
DATE: 6-10-65  
DATE:  
DATE: Oct 1965

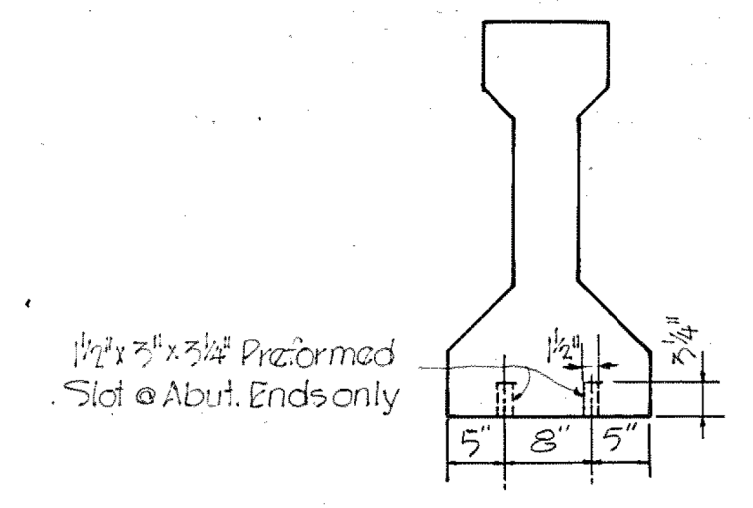
CORRECT: Fred Grove  
BRIDGE ENGINEER  
APPROVED: [Signature]  
STATE HIGHWAY ENGINEER

K-51-118

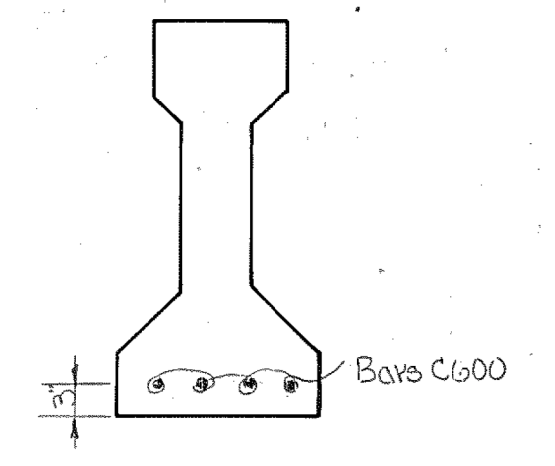
MICROFILMED



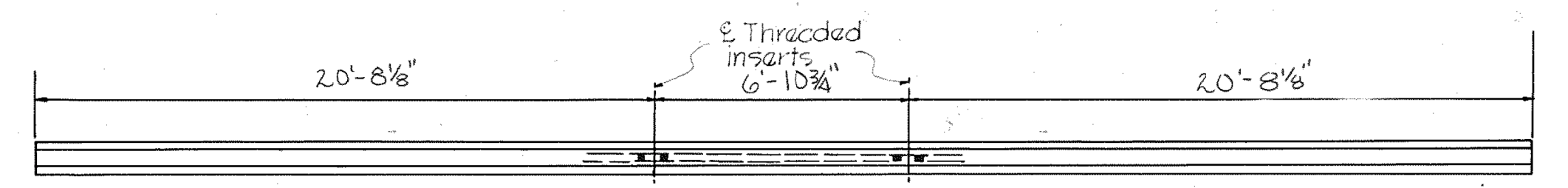
ELEVATION  
Showing Strand Deflection Pattern



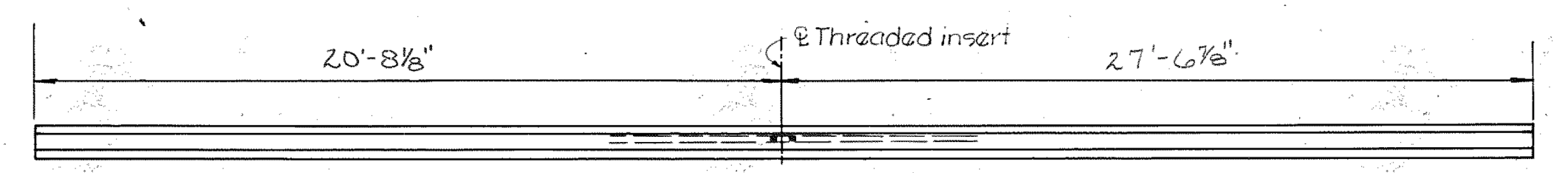
DETAIL "A"  
Typical of Abutments only



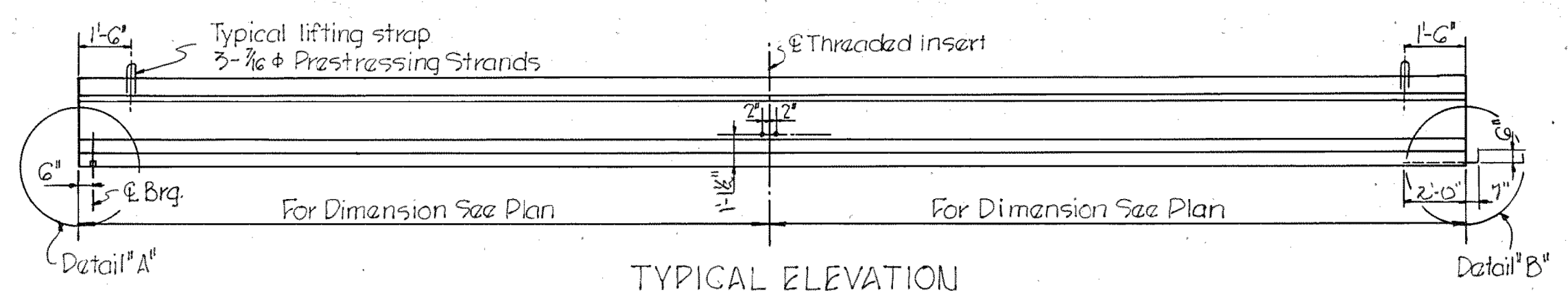
DETAIL "B"  
Typical of Fixed Ends



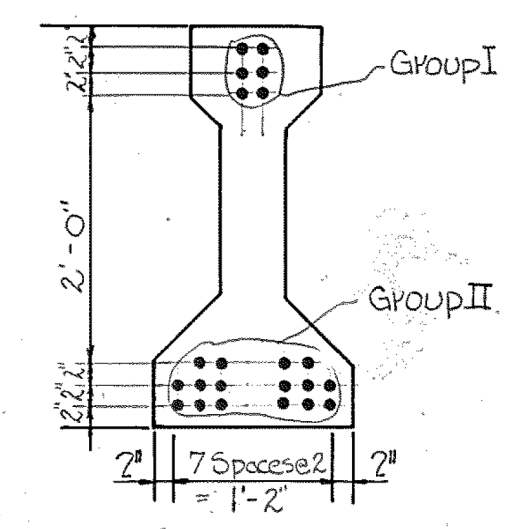
PLAN OF INTERIOR BEAM



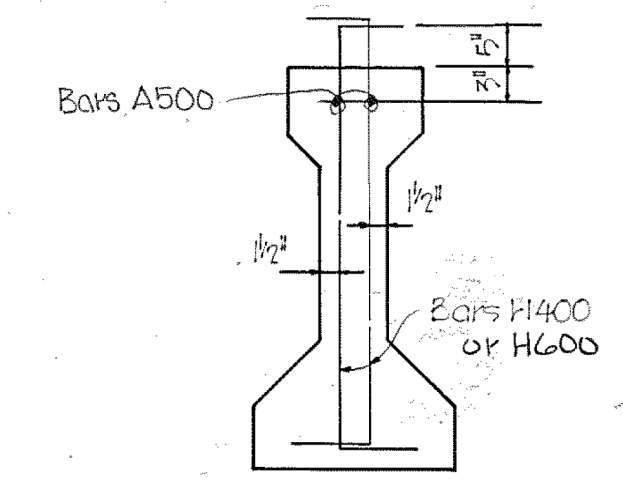
PLAN OF FASCIA BEAM



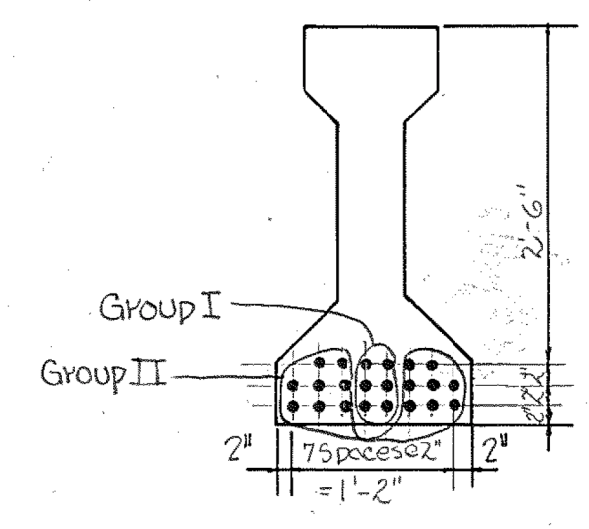
TYPICAL ELEVATION



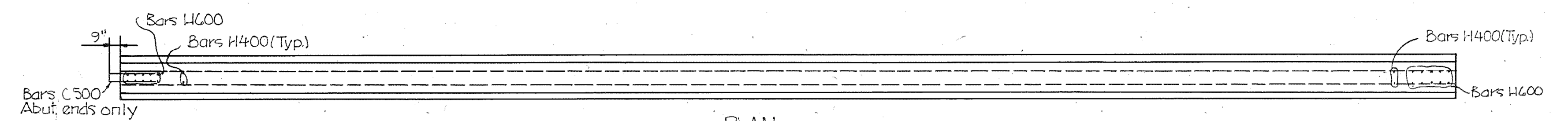
SECTION A-A



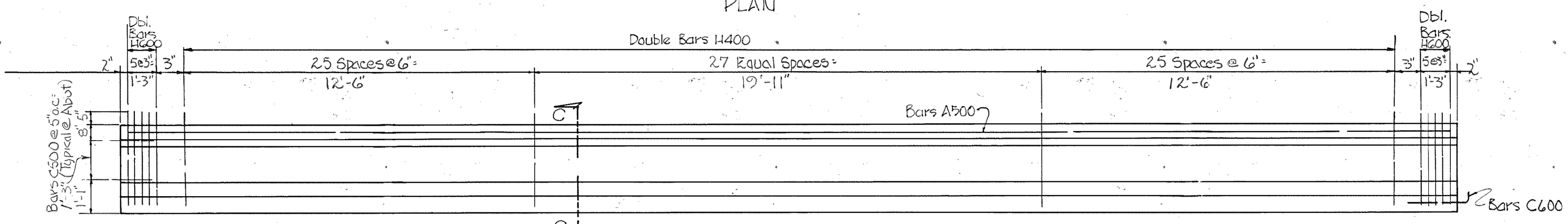
SECTION C-C  
Web Reinforcement



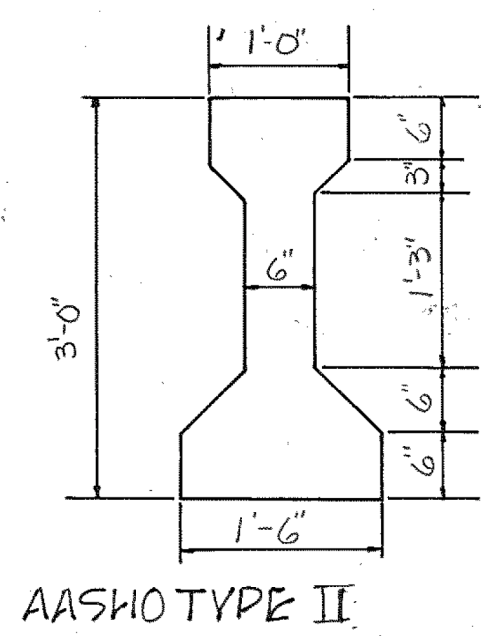
SECTION B-B



PLAN



ELEVATION  
Showing Mild Reinforcing Steel



AASHTO TYPE II

GENERAL NOTES

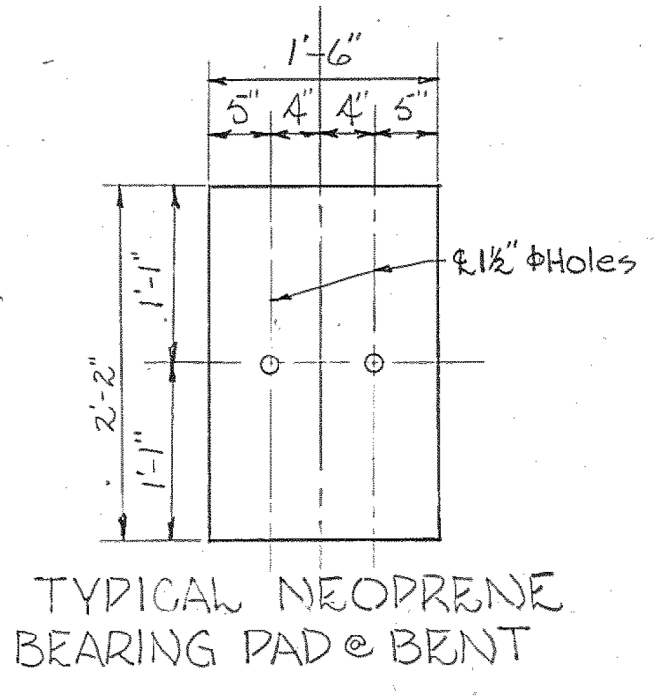
1. THE TOP OF ALL BEAMS IS TO BE ROUGH FLOATED AT THE APPROXIMATELY THE TIME OF INITIAL SET, THE TOP OF BEAMS TO BE SCRUBBED TRANSVERSELY WITH A COARSE WIRE BRUSH TO REMOVE ALL LAITANCE AND TO PRODUCE A ROUGH SURFACE.
2. ANCHORAGE AT EXPANSION END OF BEAMS ACCOMPLISHED BY DOWELS 1"Øx1'-6" FIXED END DOWELS 1"Øx1'-9"
3. MILD REINFORCING STEEL TO BE INTERMEDIATE OR HARD GRADE.
4. ALL PRESTRESSING STRANDS TO BE 7/16"Ø 7 WIRE, TYPE 270K STRESS-RELIEVED.
5. AN INITIAL FORCE OF 21,700# SHALL BE APPLIED TO EACH STRAND IN ALL BEAMS.
6. ALL BEAMS ARE AASHTO STANDARD TYPE II.
7. AFTER THE BEAM IS REMOVED FROM THE PRESTRESS BED, BARS C600 SHALL BE BENT HORIZONTALLY A SUFFICIENT AMOUNT SO AS TO PERMIT BARS C600 OF ADJOINING BEAMS TO MESH WHEN IN AN ERECTED POSITION.
8. THE STRANDS SHALL BE LEFT PROJECTING 3" FROM THE ENDS OF THE BEAMS THERE SHALL NOT BE ANY PROTECTIVE COATING PLACED ON THE ENDS OF BEAMS OR ON PROJECTING STRANDS.

BILL OF STEEL - EACH BEAM

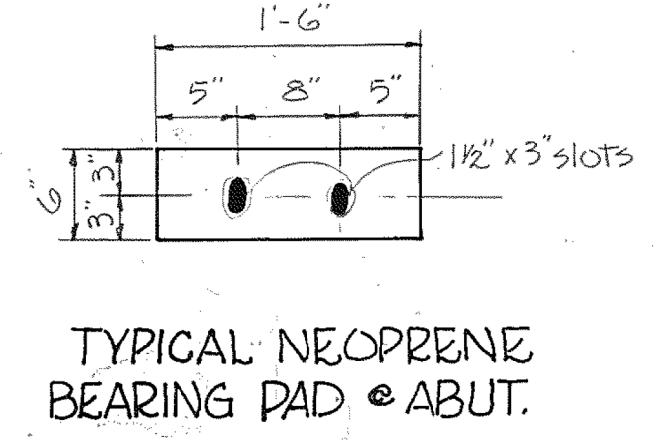
Bar	Size	SPANS 1 & 4		SPANS 2 & 3	
		No.	Length	No.	Length
A500	5	2	47'-9"	2	47'-9"
C500	5	8	2'-11"		
C600	6	4	2'-11"	8	2'-11"
H400	4	156	4'-6"	156	4'-6"
H600	6	24	4'-6"	24	4'-6"

ESTIMATED QUANTITIES - PER BEAM

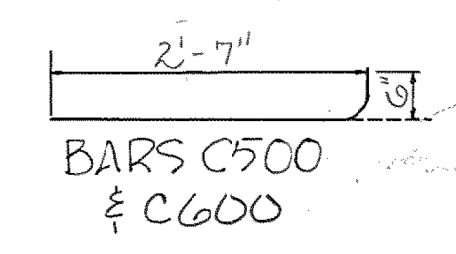
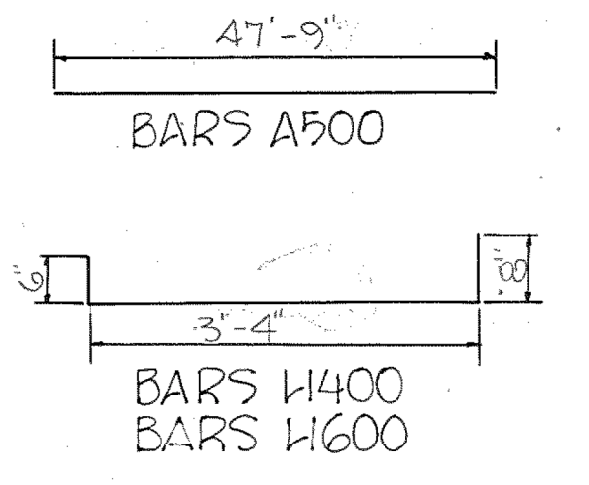
Beam	Conc. Cu Yds	Rein. Stl. lbs.	Pre. Stl. lbs.	Thrs. Inserts	Thrs. Rods
E2F1	4.6	772	425	1	2
E2F2	4.6	772	425	1	2
E2I1	4.6	772	425	2	4
I2F1	4.6	767	425	1	2
I2I1	4.6	767	425	2	4



TYPICAL NEOPRENE BEARING PAD @ ABUT.



TYPICAL NEOPRENE BEARING PAD @ BENT



STATE OF TENNESSEE  
DEPARTMENT OF HIGHWAYS  
NASHVILLE  
BEAM DETAILS  
INTERSTATE 40 OVER FRONTAGE ROAD  
& TENNESSEE CENTRAL RAILROAD  
STATION 1296+98.24  
CUMBERLAND COUNTY  
-1965-

CORRECTED: Fred Greve  
APPROVED: [Signature]  
STATE HIGHWAY ENGINEER

K-51-119

DESIGNED BY: Don Shanklin DATE: 5-65  
DRAWN BY: Robert Rowlett DATE: 6-23-65  
TRACED BY: DATE:  
CHECKED BY: Don Shanklin DATE: 10-65

MICROFILMED

Revised 2-Feb-66 - Stations Changed

NOTE: WHEN POURING ABUTMENT BEAM PROVISIONS SHALL BE MADE FOR SETTING DOWEL BARS FOR PRESTRESSED BEAMS. BAR PROJECTION - 3".

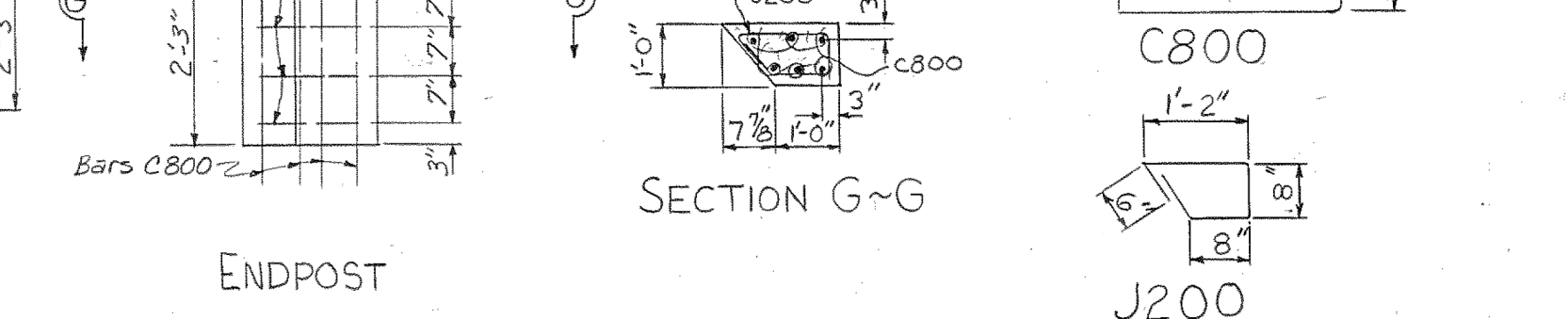
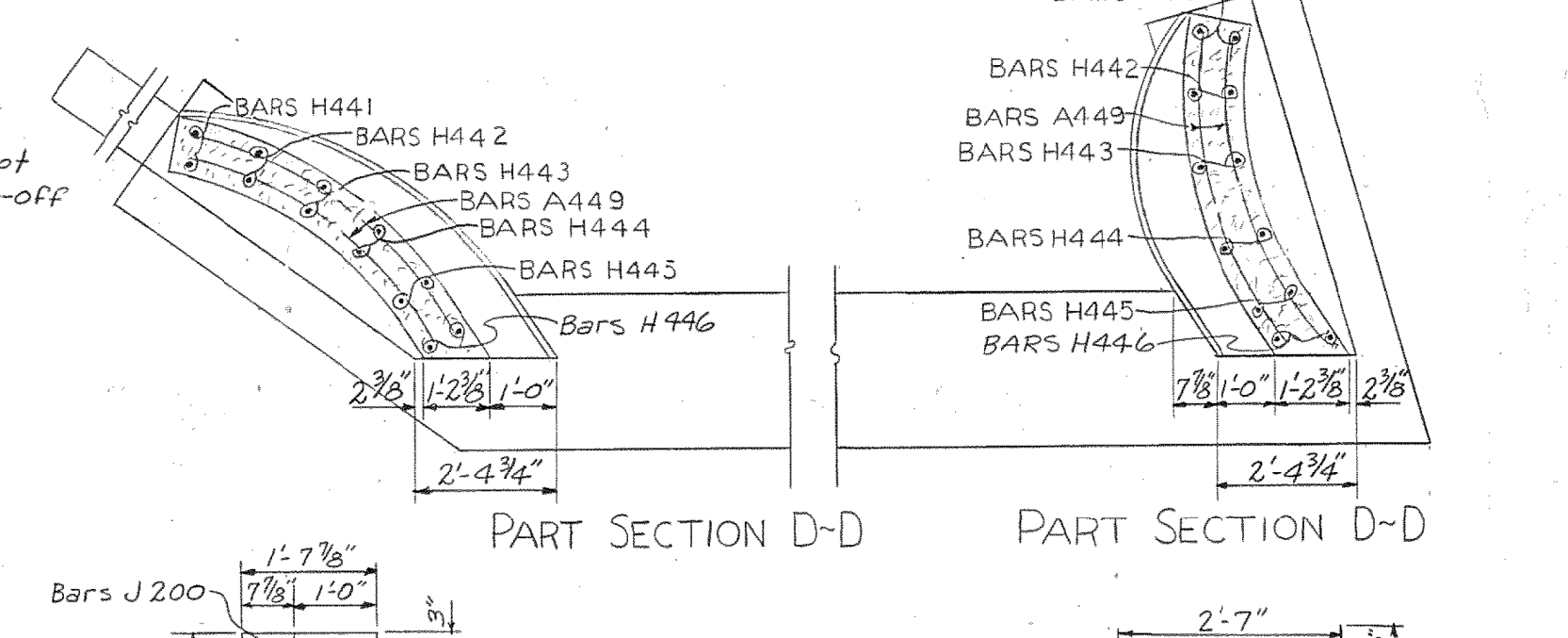
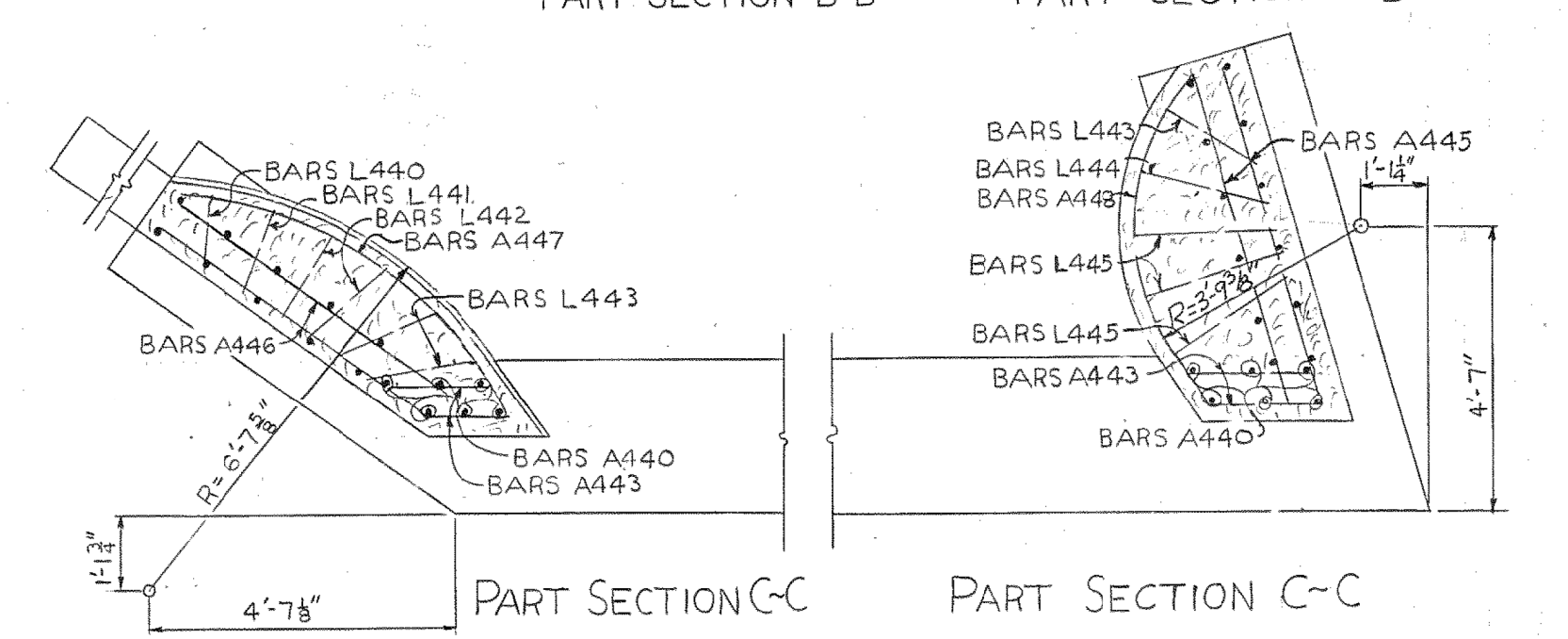
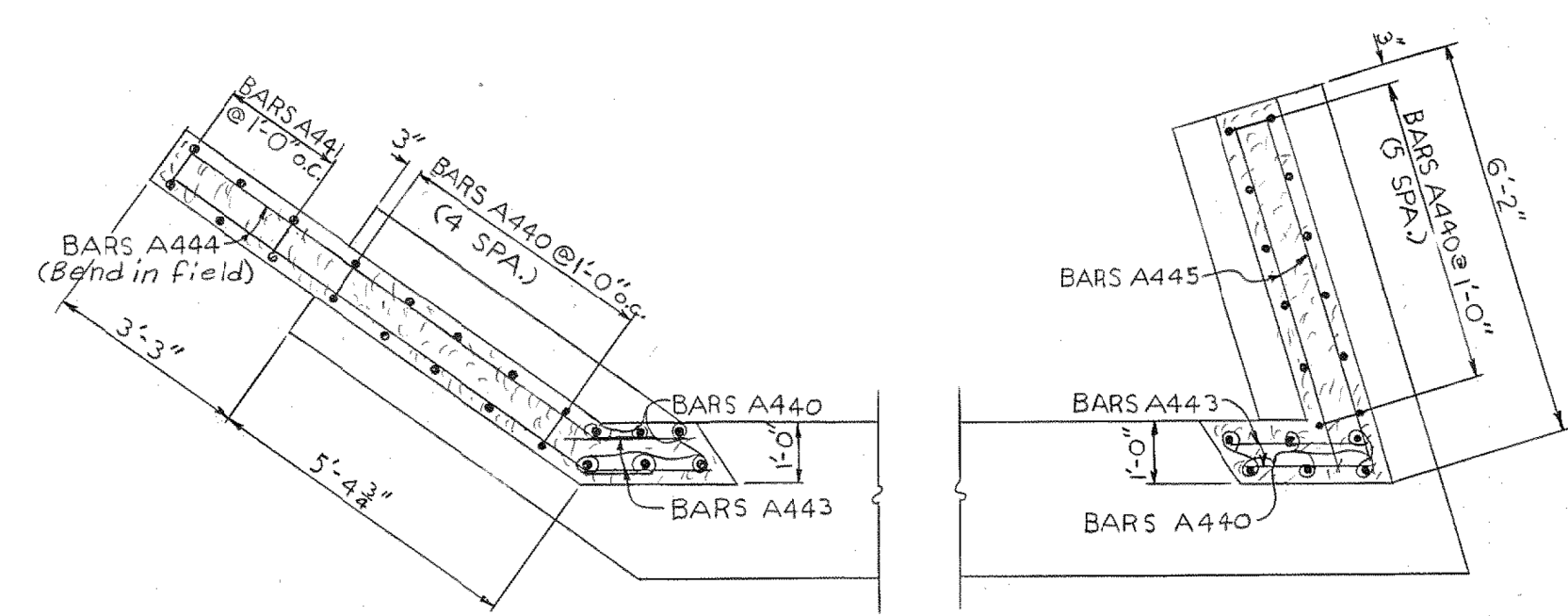
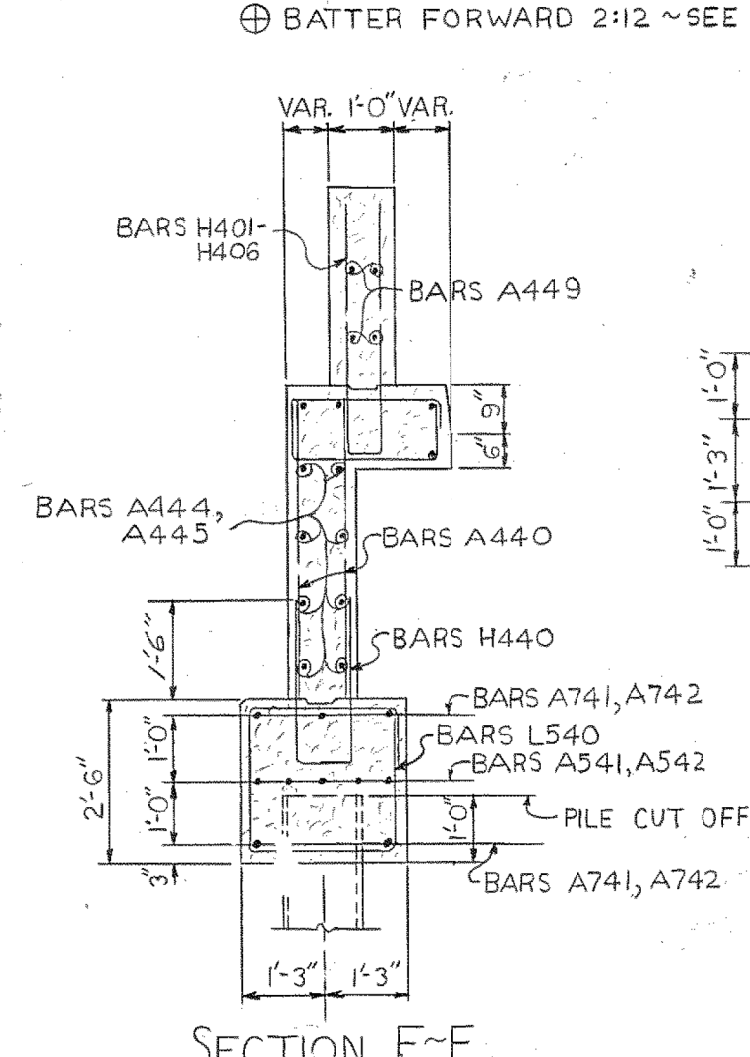
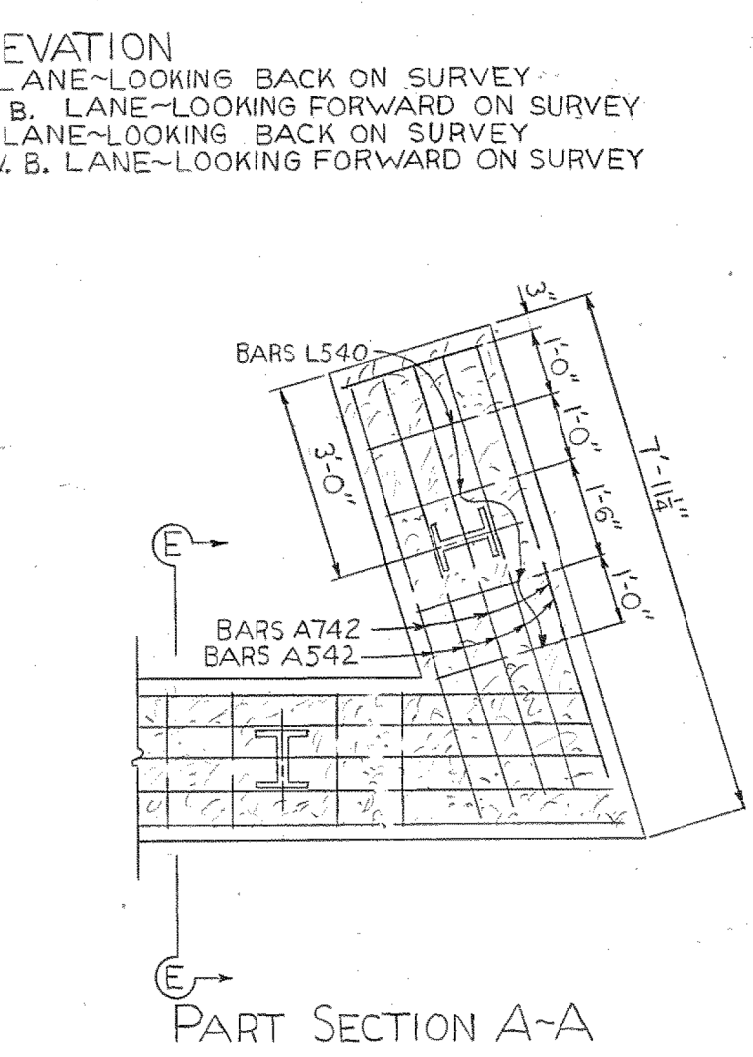
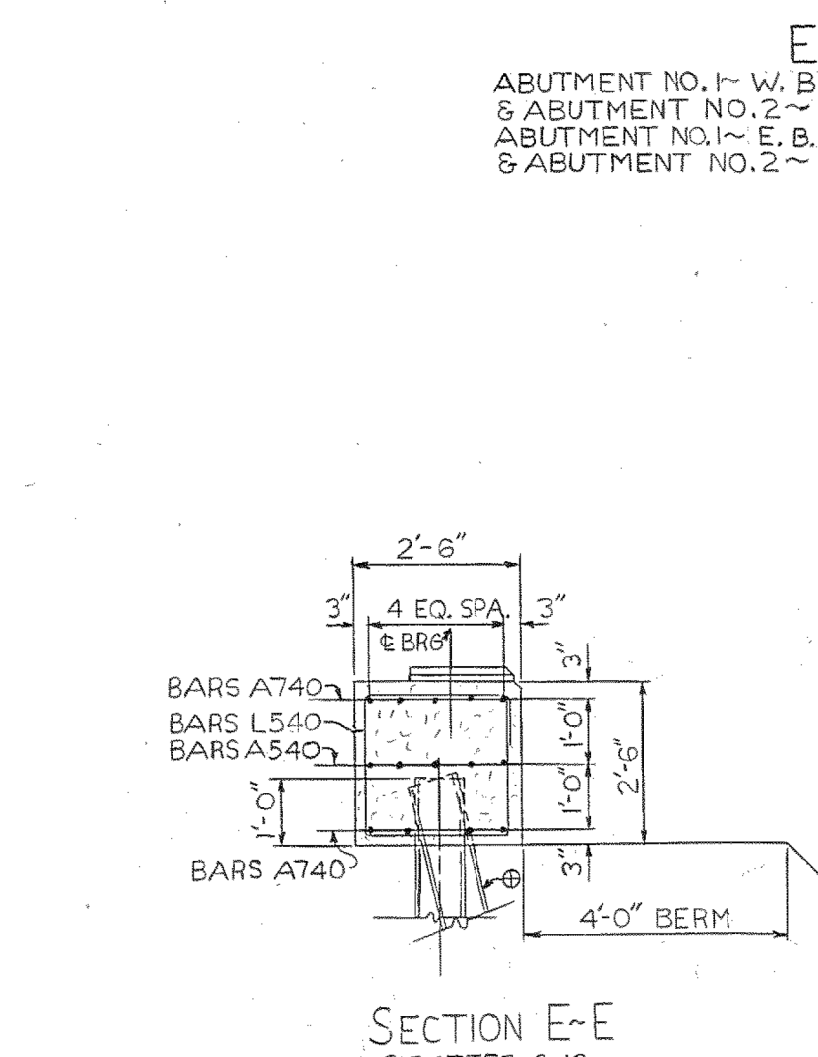
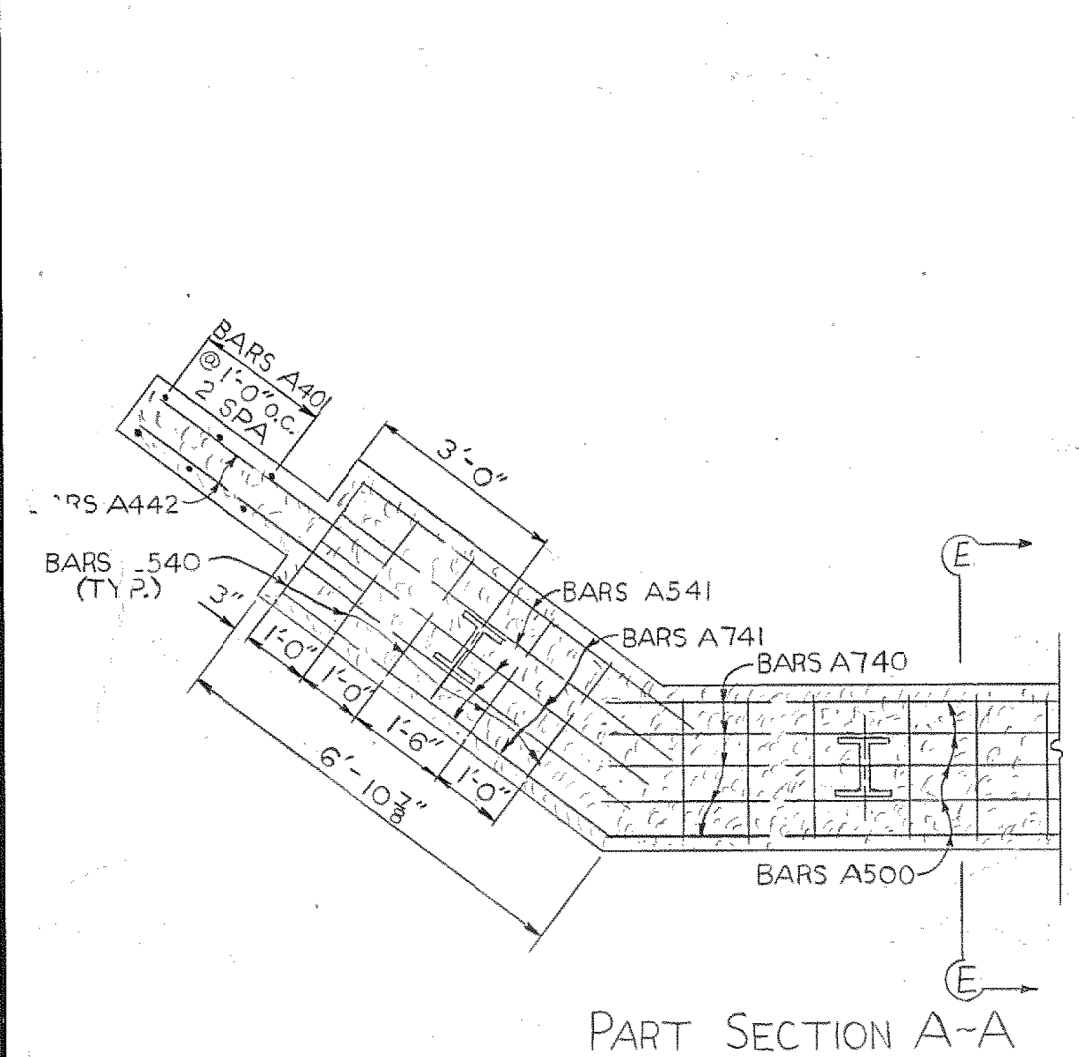
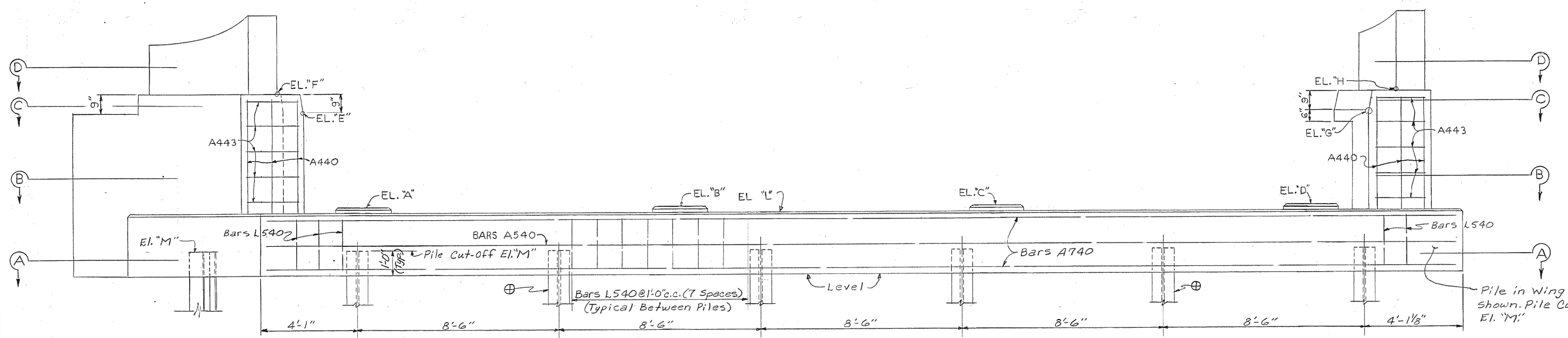
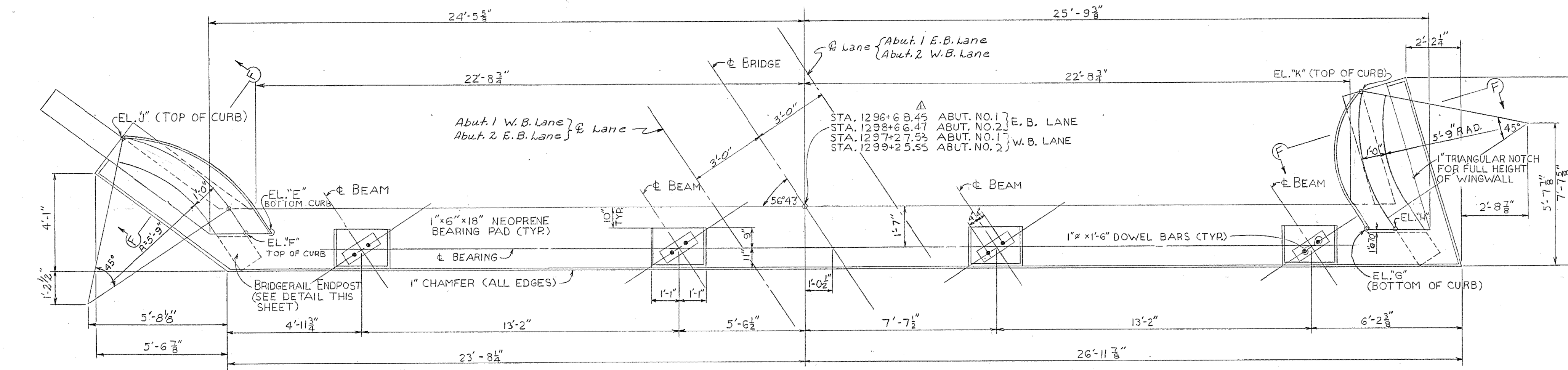


TABLE OF ELEVATIONS

	A	B	C	D	E	F	G	H	J	K	L	M
ABUT. NO. 1~E. B. LN.	1768.35	1768.39	1768.42	1768.44	1772.15	1772.90	1772.25	1772.99	1772.97	1773.03	1768.13	1766.63
ABUT. NO. 2~E. B. LN.	1765.57	1766.52	1766.47	1766.41	1770.18	1770.94	1770.38	1771.13	1770.91	1771.08	1766.23	1764.75
ABUT. NO. 1~W. B. LN.	1768.03	1767.84	1767.66	1767.46	1771.88	1772.63	1771.22	1771.96	1772.85	1772.00	1762.29	1765.79
ABUT. NO. 2~W. B. LN.	1765.81	1765.98	1766.14	1766.30	1770.13	1770.88	1769.50	1770.31	1770.90	1770.27	1765.64	1764.14

ESTIMATED QUANTITIES

ITEM	CONCRETE CLASS A-CU. YDS.	REINFORCING STEEL-LBS.
ABUT. NO. 1~E. B. LN.	18.7	2387
ABUT. NO. 2~E. B. LN.	18.7	2387
ABUT. NO. 1~W. B. LN.	18.7	2387
ABUT. NO. 2~W. B. LN.	18.7	2387

ENDPOST~LIST OF MATERIALS~EACH

BAR	SIZE	NO.	LENGTH	CONCRETE CLASS A-CU. YDS.	REINFORCING STEEL-LBS.
C800	#1	6	3'-7"		
J200	#2	4	3'-10"	.11	60

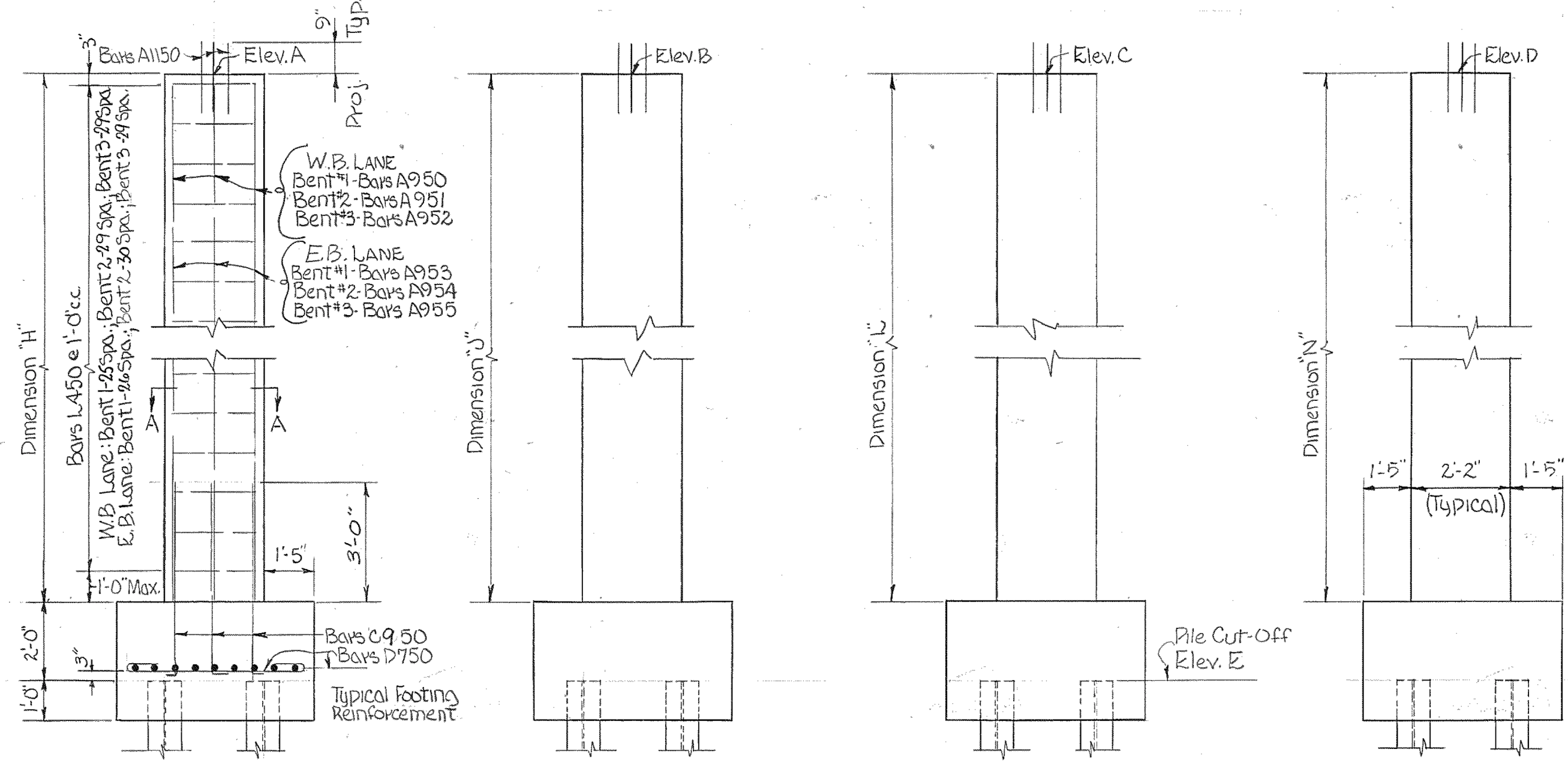
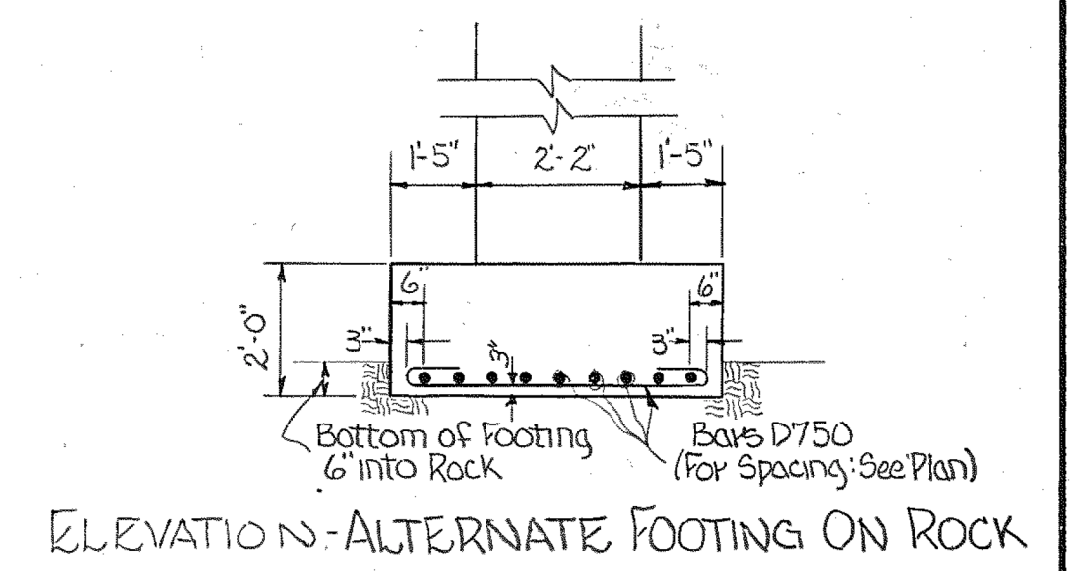
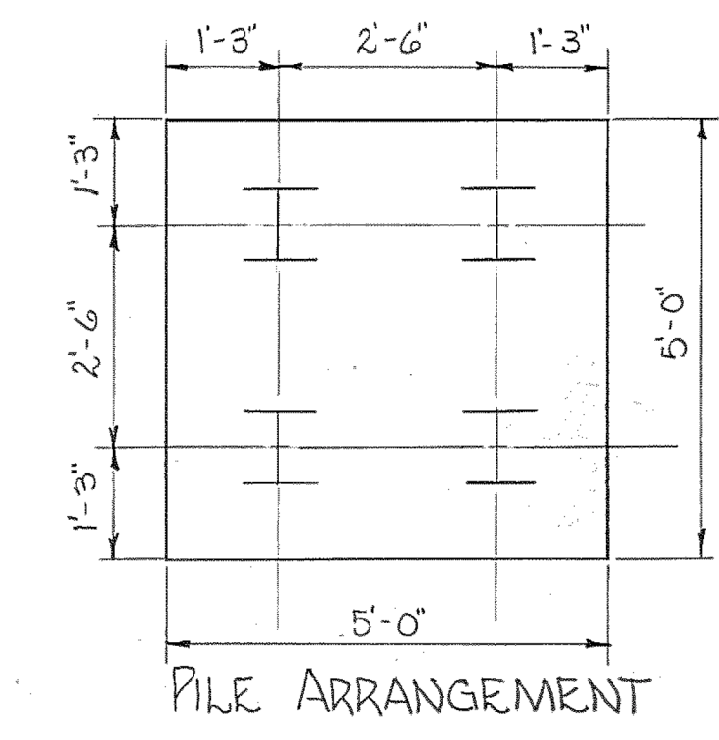
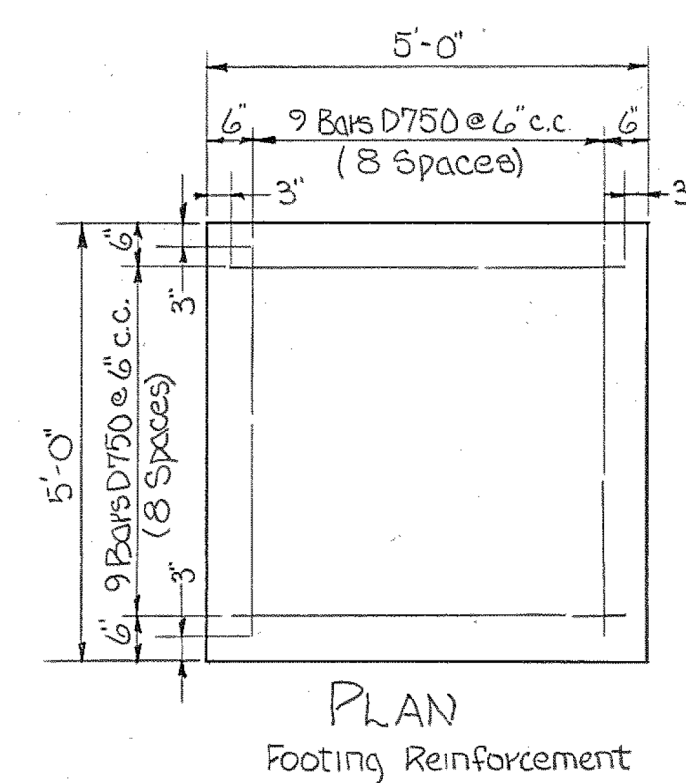
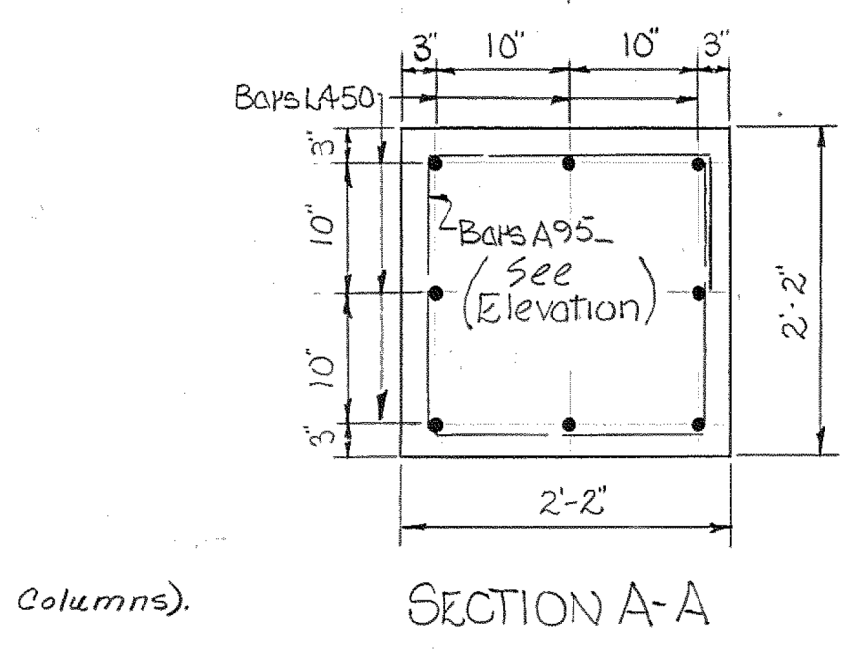
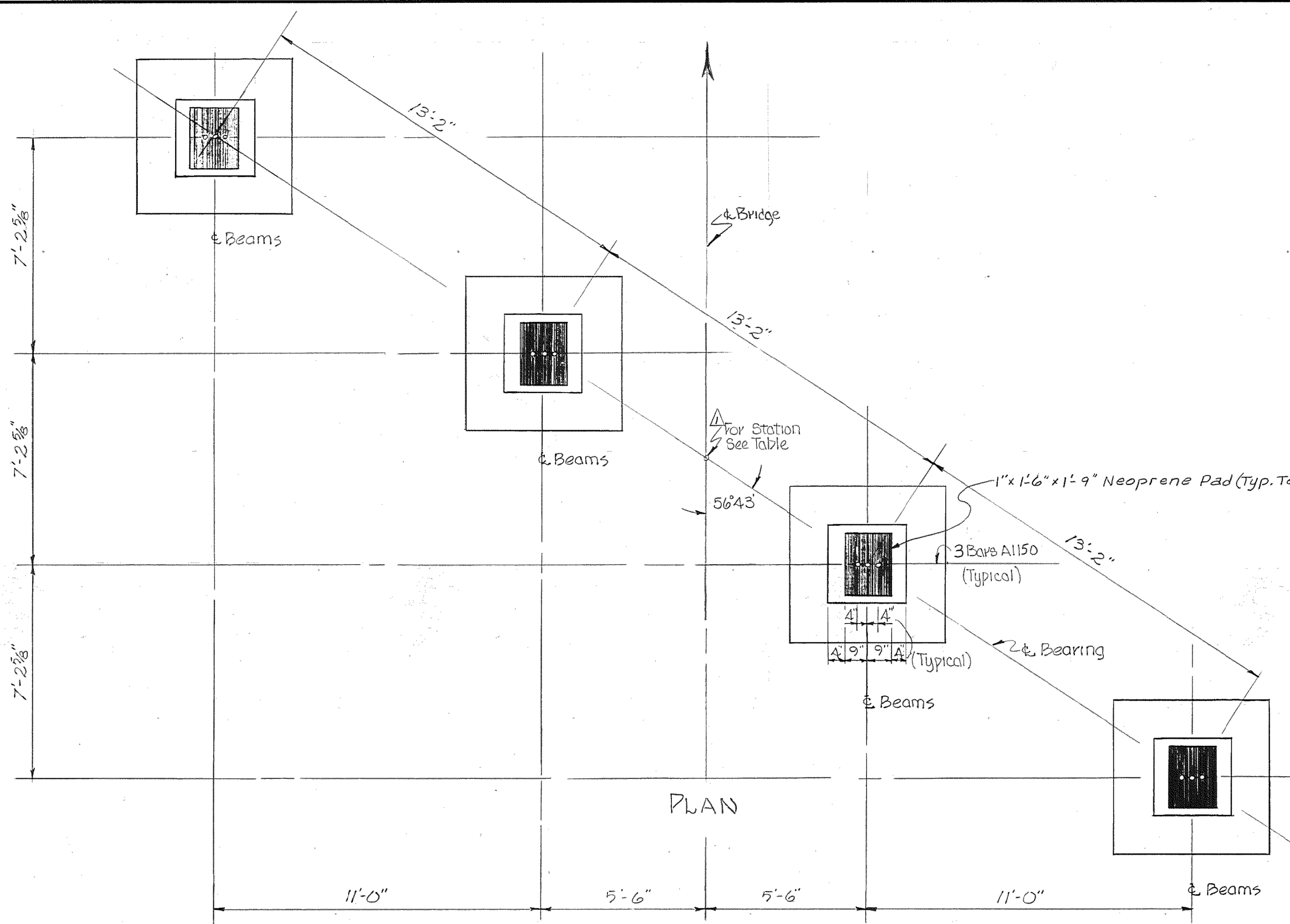
DESIGNED BY: Don Shanklin  
 DRAWN BY: JML  
 CHECKED BY: Don Shanklin

DATE: 7-8-65

STATE OF TENNESSEE  
 DEPARTMENT OF HIGHWAYS  
 NASHVILLE  
 ABUTMENT DETAILS  
 INTERSTATE 40 OVER FRONTAGE RD.  
 & TENNESSEE CENTRAL RAILROAD  
 STATION 1296+98.24  
 CUMBERLAND COUNTY  
 1965

CORRECT: [Signature]  
 BRIDGE ENGINEER  
 APPR. ED: [Signature]  
 STATE HIGHWAY ENG.

Revised 2-2-66: Station Change C.F.C.



ELEVATION OF BENTS No. 1, 2, & 3  
Looking Forward On Survey  
Typical Both Lanes

ESTIMATED QUANTITIES

WEST BOUND LANE	ITEM	Concrete Class A' Cu. Yds.	Reinforcing Steel Lbs.
	Bent No. 1	29.0	4950
	Bent No. 2	31.6	5116
	Bent No. 3	31.3	5391
EAST BOUND LANE	ITEM	Concrete Class A' Cu. Yds.	Reinforcing Steel Lbs.
	Bent No. 1	29.7	5081
	Bent No. 2	32.7	5631
	Bent No. 3	32.0	5500

TABLE OF STATIONS, DIMENSIONS, & ELEVATIONS

WEST BOUND LANE	ITEM	Stations	Dimensions					Elevations				
			H	J	L	N	A	B	C	D	E	
	Bent No. 1	1297+77.54	25'-9"	25'-11 1/2"	26'-1 3/4"	26'-3 1/2"	1766.98	1767.17	1767.35	1767.53	1767.73	
	Bent No. 2	1298+26.54	29'-6"	29'-8 1/2"	29'-10 1/2"	30'-0"	1766.34	1766.73	1766.90	1767.07	1767.24	
	Bent No. 3	1298+75.54	29'-0"	29'-2 1/2"	29'-4 1/2"	29'-6 1/2"	1766.15	1766.33	1766.49	1766.64	1766.81	

EAST BOUND LANE	ITEM	Stations	Dimensions					Elevations				
			H	J	L	N	A	B	C	D	E	
	Bent No. 1	1297+18.46	26'-10 1/2"	26'-10"	26'-9 1/2"	26'-9"	1767.91	1767.80	1767.84	1767.80	1767.85	
	Bent No. 2	1297+67.46	31'-1 1/2"	31'-1 1/2"	31'-0 1/2"	31'-0"	1767.42	1767.30	1767.34	1767.29	1767.34	
	Bent No. 3	1298+16.46	30'-1 1/2"	30'-1 1/2"	30'-1 1/2"	30'-0 1/2"	1766.97	1766.92	1766.87	1766.82	1766.84	

MICROFILMED

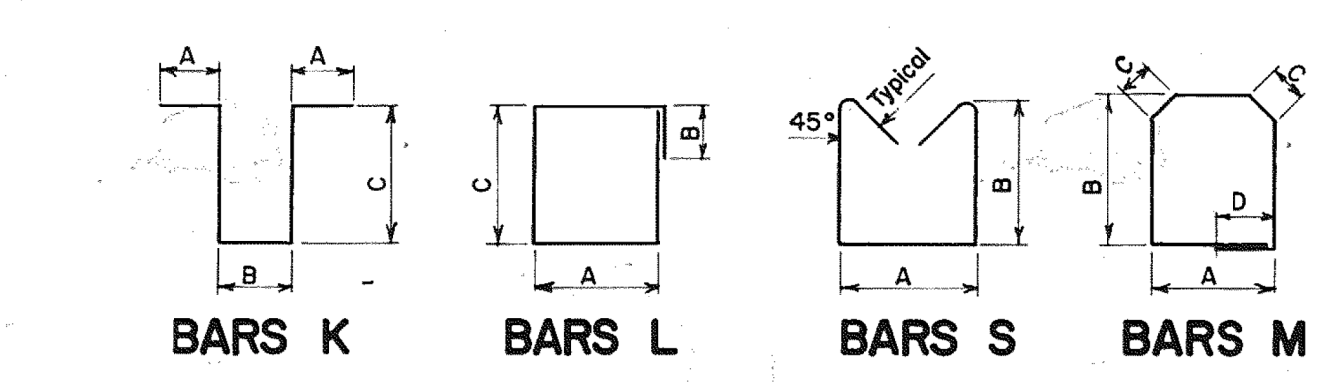
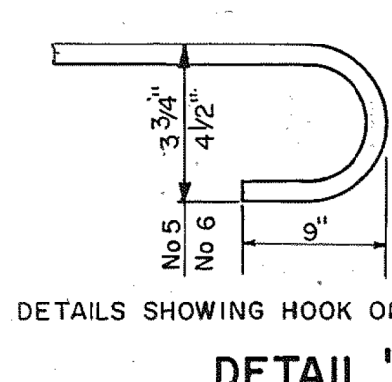
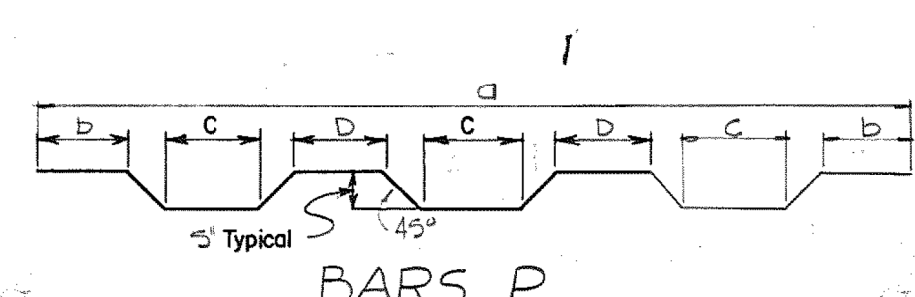
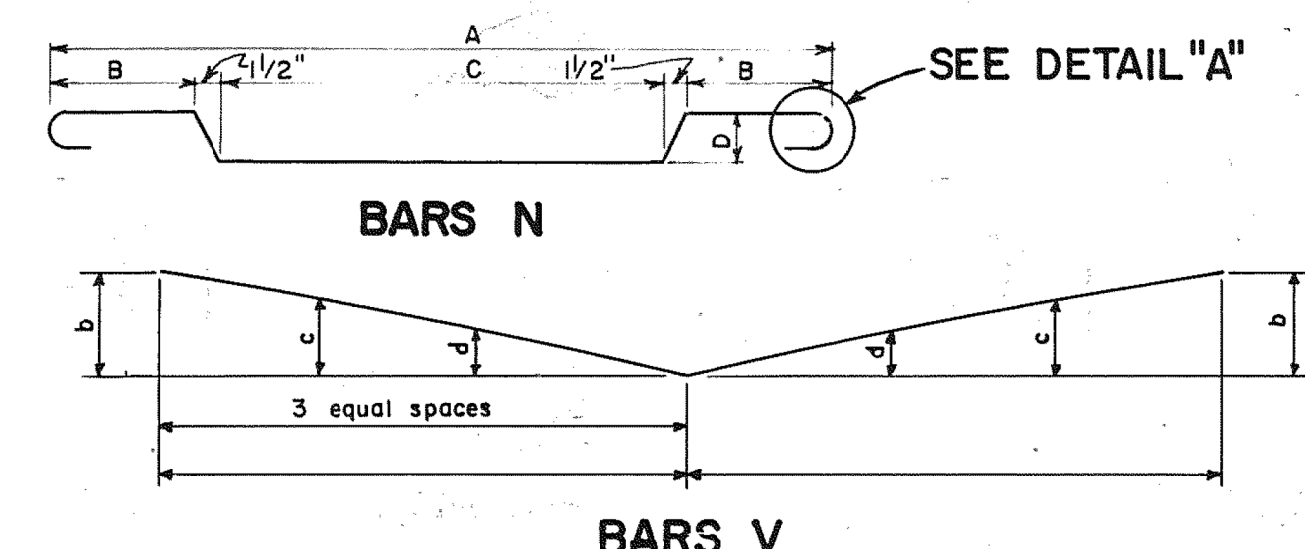
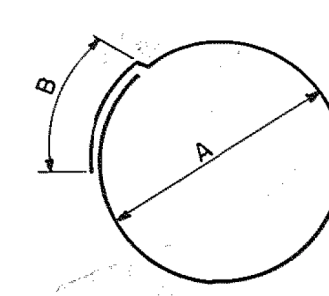
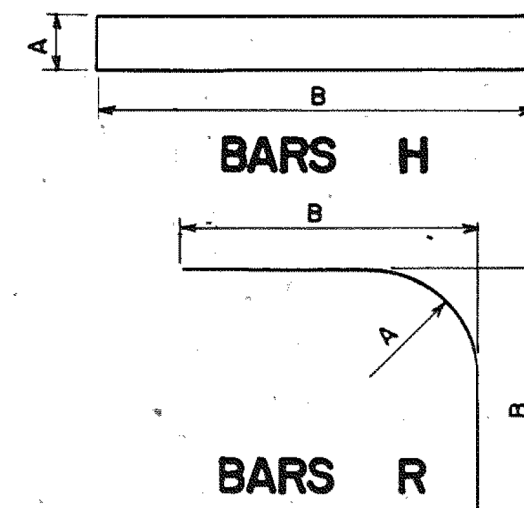
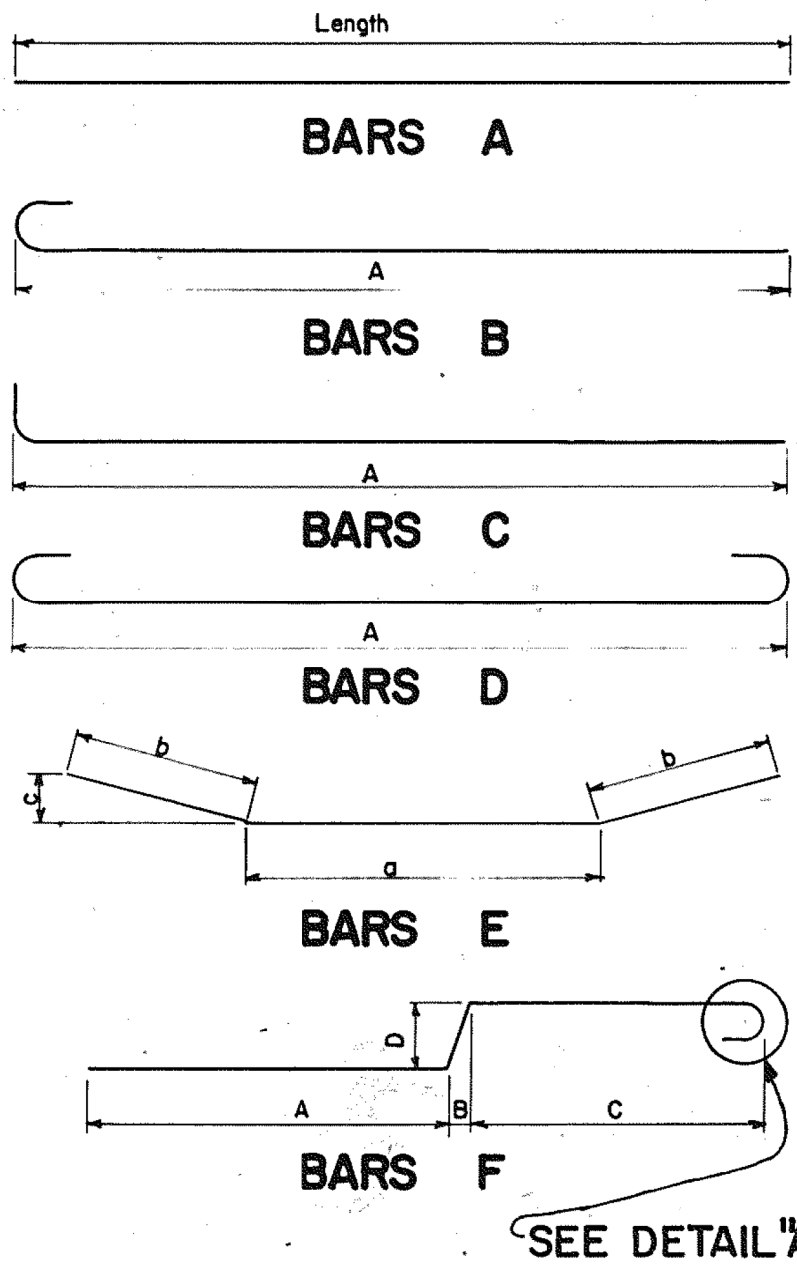
DESIGNED BY Don Shanklin DATE 5-65  
 DRAWN BY R.E. Rowlett DATE 6-16-65  
 TRACED BY DATE  
 CHECKED BY Don Shanklin DATE 5-65

STATE OF TENNESSEE  
 DEPARTMENT OF HIGHWAYS  
 NASHVILLE  
 BENT DETAILS  
 INTERSTATE 40 OVER FRONTAGE ROAD &  
 TENNESSEE CENTRAL RAILROAD  
 STATION 1296+98.24  
 CUMBERLAND COUNTY  
 -1965-

CORRECT *Fred Grove*  
 BRIDGE ENGINEER  
 APPROVED *John Dunlap*  
 STATE HIGHWAY ENGINEER

K-51-121

SUPERSTRUCTURE						ABUTMENTS NO. 1&2						BENTS NO. 1,2&3										
Bar	Location	Size	No. Req'd	Bending Dimensions		Length	Bar	Location	Size	No. Req'd	Bending Dimensions		Length	Bar	Location	Size	No. Req'd	Bending Dimensions		Length		
				A	B						A	B	C	D				A	B	C	D	
A500	SLAB	#5	330			24'-3"	A140	WINGWALL	#4	28			4'-6"	A950	COLUMN BT.1	#9	32					7'-5"
A501	INTER DIAPH.	#5	24			9'-9"	A141	WINGWALL EXT.	#4	12			6'-3"	A951	COLUMN BT.2	#9	32					9'-3"
A502	SLAB DIAPH.	#5	36			14'-6"	A142	WINGWALL EXT.	#4	12			4'-3"	A952	COLUMN BT.3	#9	32					28'-3"
A503	ENDWALL	#5	16			4'-0"	A143	WINGWALL	#4	40			2'-0"	C950	FOOTINGS	#9	36	4'-0"				2'-5"
A600	SLAB	#6	170			41'-0"	A144	WINGWALL	#4	16			9'-3"	D750	FOOTINGS	#7	210	4'-6"				6'-0"
A601	CURB	#6	48			34'-2"	A145	WINGWALL	#4	20			5'-0"	L450	COLUMNS	#4	344	1'-0"	1'-0"	1'-0"		8'-2"
A602	SLAB	#6	70			39'-3"	A146	CURB-LEFT SIDE	#4	4			5'-0"	A150	Columns	#11	36					1'-9"
A603	SLAB	#6	8			4'-6"	A147	CURB-LEFT SIDE	#4	4			6'-6"									
H500	ROADWAY BRACK	#5	30	1'-5"	6"	7'-5"	A148	CURB-RIGHT SIDE	#4	4			5'-9"									
H400	SLAB DIAPH.	#4	108	6'-4"	4'-2"	3'-2"	A449	WINGPOST	#4	16			5'-3"									
H401	INTER DIAPH.	#4	120	6'-4"	4'-2"	1'-2"	A540	ABUT. BEAM	#3	10			50'-0"									
H402	ENDWALL	#4	30	4'-2"	6'-4"	3'-6"	A741	ABUT. BEAM	#7	18			50'-0"									
U600	SLAB & Curb	#6	170	4'-6"	1'-6"	38'-2"	A741	ABUT. BEAM	#7	10			6'-6"									
U601	SLAB	#6	169	4'-6"	6'-8"	4'-11"	A742	ABUT. BEAM	#7	10			7'-0"									
F600	Curb	#6	52	2'-0"	1'-2"	1'-2"	H440	BEAM & WINGWALL	#4	34			2'-6"									
Series	LOCATION	SIZE	WT. OF SERIES	WT. OF SERIES		TOTAL WT.	H441	WINGPOST	#4	4	8	2'-0"	4'-6"									
A610	SLAB	#6	4	888'-8"	15'-3"	5332'	H442	WINGPOST	#4	4	8	2'-14"	4'-8"									
A800	Slab	#8	48			24'-1"	H443	WINGPOST	#4	4	8	2'-14"	5'-0"									
A801	"	#8	48			45'-0"	H445	WINGPOST	#4	4	8	2'-14"	6'-0"									
A802	"	#8	36			18'-0"	H446	WINGPOST	#4	4	8	3'-0"	6'-6"									
A803	"	#8	48			26'-11"	L440	CURB	#4	2	1'-0"	6'-4"	11"	4'-3"								
A804	Slab	#8	24			30'-0"	L441	CURB	#4	2	1'-0"	6'-4"	11"	4'-11"								
							L442	CURB	#4	2	1'-0"	6'-4"	11"	5'-3"								
							L443	CURB	#4	2	1'-0"	6'-4"	11"	5'-9"								
							L444	CURB	#4	2	2'-0"	6'-4"	11"	6'-3"								
							L445	CURB	#4	2	2'-0"	6'-4"	11"	6'-9"								
							L540	BEAM	#5	110	2'-2"	1'-0"	2'-2"	9'-6"								
							A541	WING. BRK.	#5	10			6'-6"									
							A542	WING. BRK.	#5	10			7'-0"									



**REINFORCING STEEL CODE**

Type	Size	Series
A	5	06

NOTE: Dimensions shown on this sheet are outside to outside of bar.

STATE OF TENNESSEE  
 DEPARTMENT OF HIGHWAYS  
 NASHVILLE  
**BILL OF STEEL**  
 INTERSTATE 40 OVER FRONTAGE  
 ROAD & TENNESSEE CENTRAL RAILROAD  
 STATION 1296+98.24  
 CUMBERLAND COUNTY  
 1965

DESIGNED BY Don Shanklin  
 DRAWN BY MIKE BOSSAS  
 TRACED BY M.S. & J.B. Contrell  
 CHECKED BY Don Shanklin

DATE May, 1965  
 DATE Nov. -14- 60  
 DATE Oct. 1965

CORRECT  
 Fred Crowl  
 BRIDGE ENGINEER  
 APPROVED  
 J. J. Dunlap  
 STATE HIGHWAY ENGINEER

K-51-122

MICROFILMED